

NOTICE OF DECISION
C.S. Lewis Academy – New Modular Building
Site Design Review – DR223-0002/ADJC23-0004 – 1605 N College Street

July 18, 2023

Daniel Danicic, PE
Yamhill Land Development Services, LLC.
PO Box 1042
Newberg, OR 97132
Sent via email to djd.ylds@gmail.com

Dear Mr. Danicic,

The Community Development Director has approved the site design review and code adjustment application DR223-0002/ADJC23-0004 to place a modular classroom unit at C.S. Lewis Academy which is located at 1605 N College Street. The decision will become effective on August 1, 2023, unless an appeal is filed.

You may appeal this decision to the Newberg Planning Commission within 14 calendar days of this decision in accordance with Newberg Municipal Code (NMC) 15.100.170. All appeals must be in writing on a form provided by the Planning Division. If you wish to appeal, you must submit the written appeal form together with the required fee of \$582 (plus 5 percent technology fee) to the Planning Division within 14 days of the date of this decision.

The deadline for filing an appeal is 4:30 pm on July 31, 2023.

Site design review and code adjustment approval are only valid for one year from the effective date above. If approval for your project is approaching its expiration date, contact the Planning Division regarding extension opportunities.

If you have any questions, please contact me at 503-554-7728 or clay.downing@newbergoregon.gov.

Sincerely,



Clay Downing, Planning Manager
City of Newberg, Community Development Department

STAFF REPORT

C.S. Lewis Academy – New Modular Building Site Design Review – DR223-0002/ADJC 23-0004 – 1605 N College Street

FILE NO: DR223-0002
REQUEST: Install a 1,792 square foot modular classroom at an existing school
LOCATION: 1605 N College Street
TAX LOT: R3218AD 06900
R3218AD 07000
R3218AD 07100
APPLICANT: Daniel Danicic, PE (Yamhill Land Services, LLC)
OWNER: Pacific Region Open Bible Standard Churches
ZONE: R-1 (Low Density Residential)
SUBDISTRICTS: Airport Overlay Subdistrict

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Section I: Application Information
C.S. Lewis Academy – New Modular Building
Site Design Review – DR223-0002/ADJC23-0004 – 1605 N College Street

A. DESCRIPTION OF APPLICATION:

Daniel Danicic with Yamhill Land Services, LLC, (Applicant) has applied for a Type II site design review and code adjustment on behalf of the C.S. Lewis Academy to place a modular building on an existing school campus. The C.S. Lewis Academy provides kindergarten through 12th grade educational instruction at a ±3.46 acre campus located north of E Pinehurst Drive and west of N College Street. The campus includes two primary buildings, gymnasium, portable classroom, covered play area, and 88 off-street parking stalls. Ingress and egress for the is provided by two one-way driveway connections onto N College Street.

The proposed project includes installation of a ±1,792 square-foot modular building which will provide additional classroom space for middle school instruction. The building will be located between the existing portable classroom and covered play area which are located near the center of the site. Related to installation of the new modular classroom and as part of stormwater management for the site, the Applicant will direct stormwater from the proposed modular classroom to an existing low-impact development approach (LIDA) rain garden near the northern property boundary which will be expanded as part of this project.

In addition to the proposed modification to the site, the Applicant is requesting a code adjustment to the minimum parking standards which will allow the existing number and configuration of parking stalls to be maintained.

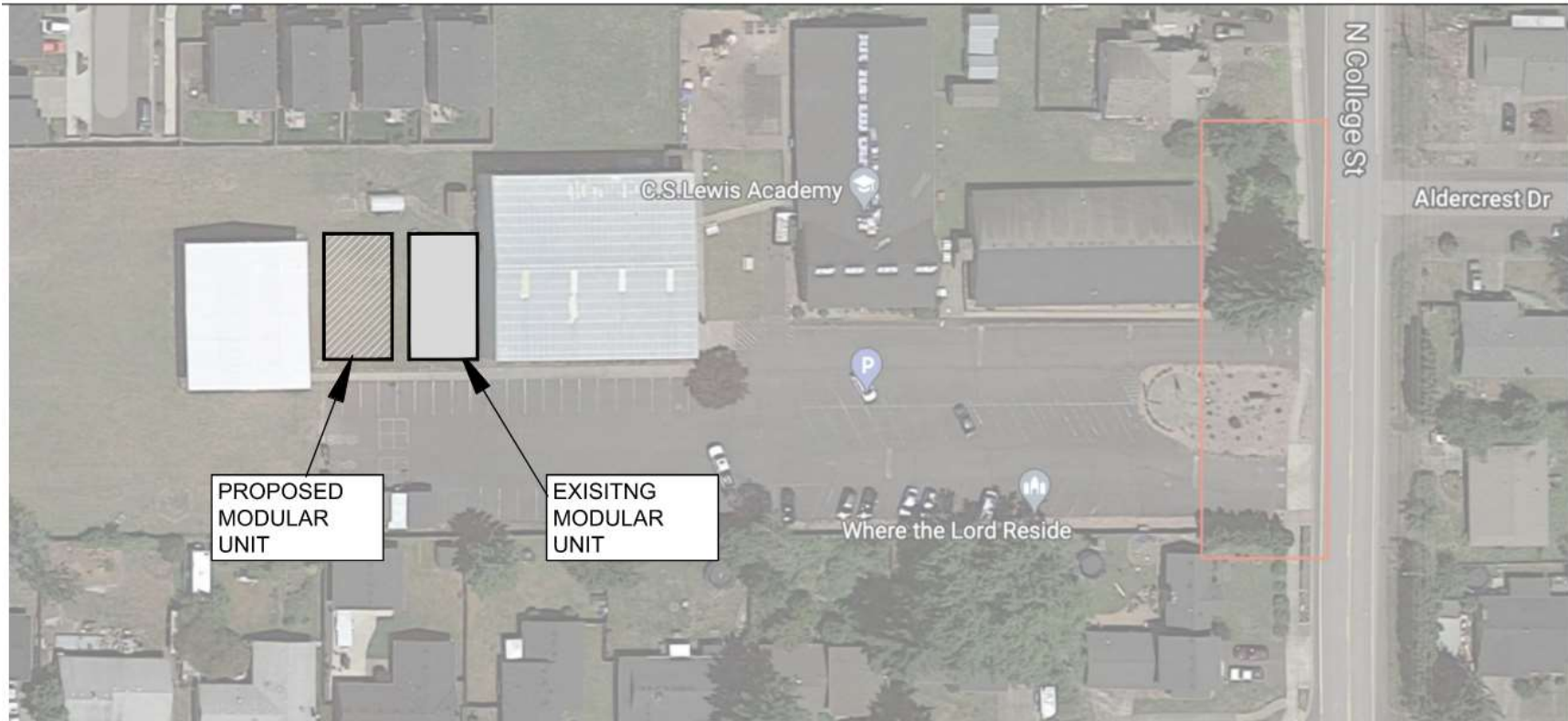
Vicinity Map



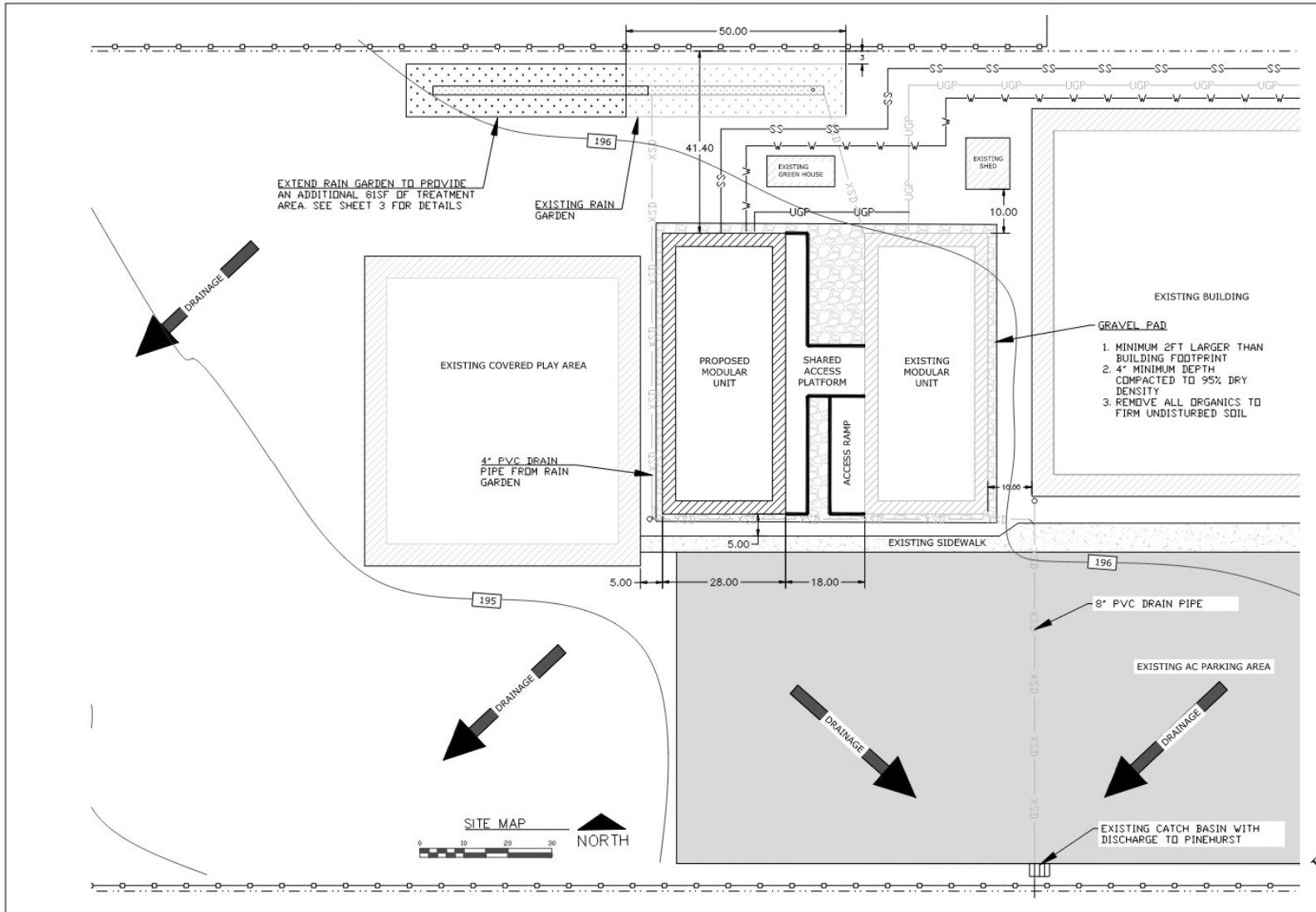
29.0

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Site Map



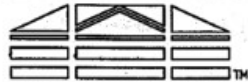

Site Plan

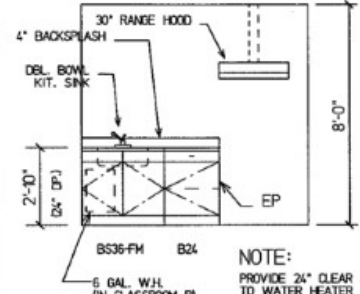


CS LEWIS ACADEMY 1605 N. COLLEGE ST. NEWBERG, OR 97132	
MODULAR BUILDING ADDITION SITE IMPROVEMENTS	
NO. DATE 01 10/17/23	REVISIONS DESCRIPTION PREP 2 APPLICATION
PREPARED BY: DANIEL DANNO DEVELOPMENT SERVICES PO BOX 1002 NEWBERG, OR 97132 503-478-7702 DAN@DANNO.COM	
PRELIMINARY EXPIRES 12/31/23	
SHEET C1	

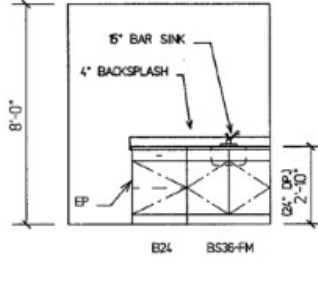
Plot Date: Mar 20, 2023 11:38 AM, /Users/70320/Projects/23/1605 N. College St. Plan Set.dwg, Layer: 01

Building Elevations

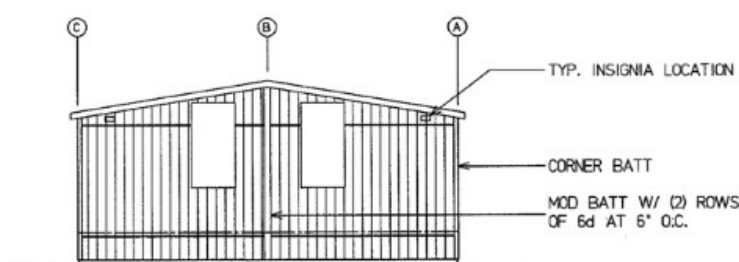
 <p>MODERN BUILDING SYSTEMS, INC. TELEPHONE: 503 749-4949 FAX: 503 749-6950 P.O. BOX 10 • 9493 PORTER ROAD • ALMSVILLE, OR 97325</p> <p>REUSE OF DOCUMENTS <small>THIS DOCUMENT AND THE IDEAS AND DESIGN INCORPORATED HEREIN IS THE PROPERTY OF MODERN BUILDING SYSTEMS, INC. AND IS NOT TO BE LOANED, REPRODUCED, COPIED, OR IN ANY MANNER, FOR ANY OTHER USE OR PROJECT WITHOUT WRITTEN AUTHORIZATION.</small></p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>REV. #</td> <td>BY:</td> <td>DATE:</td> <td>PER:</td> <td>SHEETS:</td> </tr> <tr> <td></td> <td>JGM</td> <td>9/12/01</td> <td>-</td> <td>PRELIM.</td> </tr> <tr> <td></td> <td>JGM</td> <td>9/17/01</td> <td>CUST.</td> <td>8,10</td> </tr> </table>	REV. #	BY:	DATE:	PER:	SHEETS:		JGM	9/12/01	-	PRELIM.		JGM	9/17/01	CUST.	8,10	<p>28' x 64' MODULAR CLASSROOM NEWBERG SCHOOL DISTRICT</p> <p>NEWBERG, OREGON</p> <p>EXTERIOR AND INTERIOR ELEVATIONS</p>	 <p>SHEET 9 OF 18 2001-KM-47-1</p>
REV. #	BY:	DATE:	PER:	SHEETS:														
	JGM	9/12/01	-	PRELIM.														
	JGM	9/17/01	CUST.	8,10														



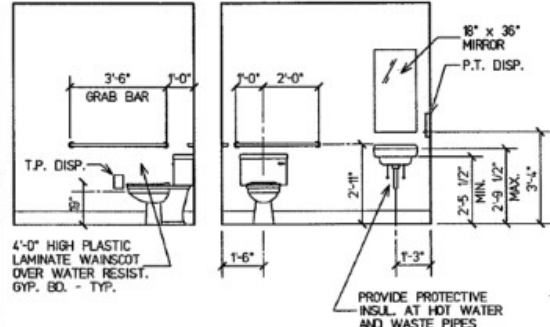
1 TYP. ELEVATION
 SCALE: 1/4" = 1'-0"



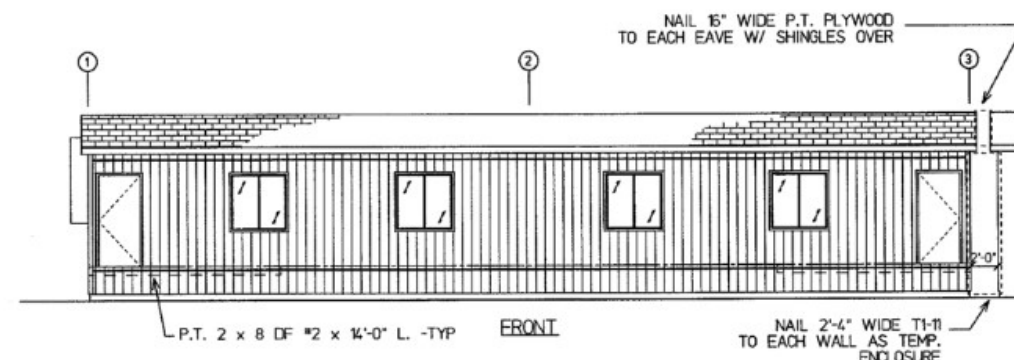
2 TYP. ELEVATION
 SCALE: 1/4" = 1'-0"



TYP. END

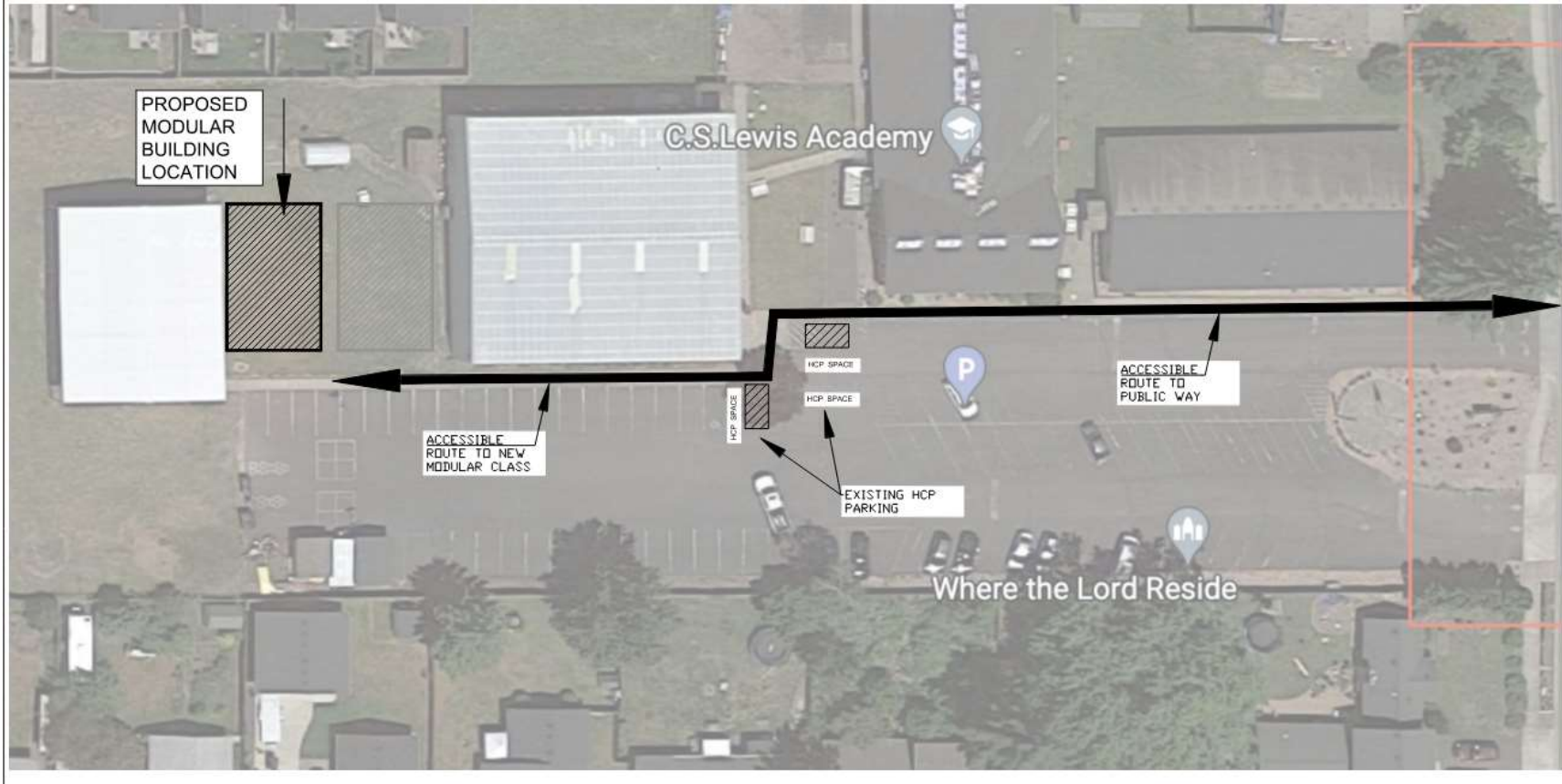


3 TYP. RESTROOM ELEVATION
 SCALE: 1/4" = 1'-0"



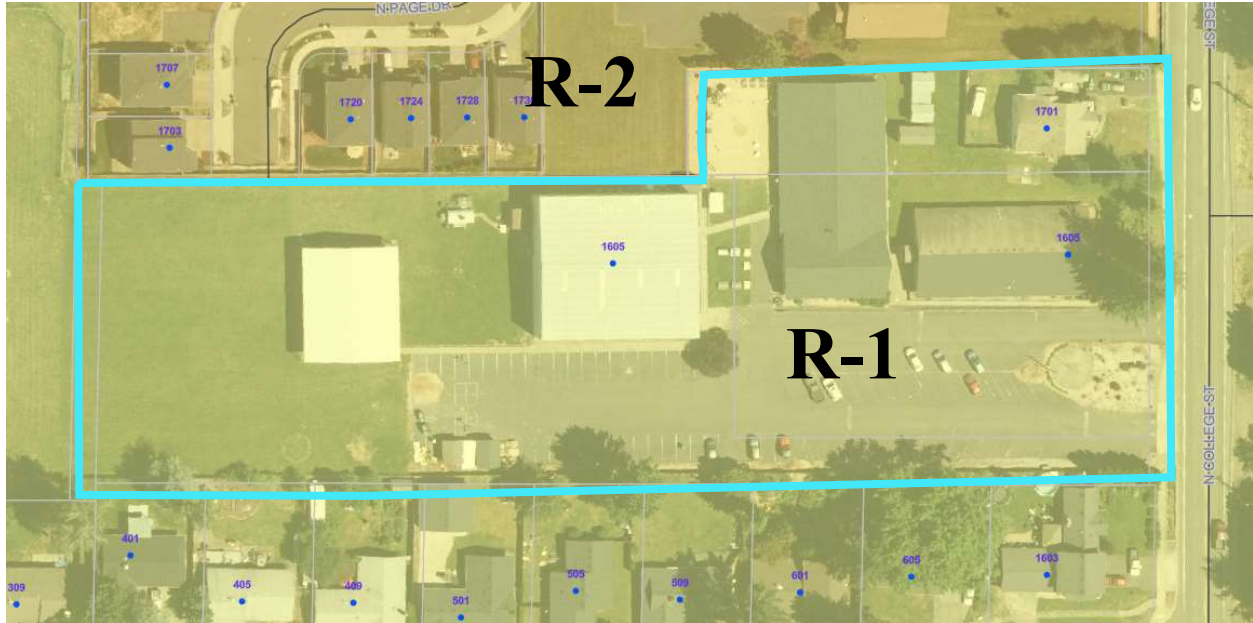
EXTERIOR ELEVATIONS
 SCALE: 1/8" = 1'-0"

Accessibility Route



B. SITE INFORMATION:

1. Location: The subject site is in the Low Density Residential (R-1) zone, located north of E Pinehurst Drive and west of N College Street. The site is occupied by a school campus.



2. Size: ±3.46 acres (±150,717 square feet)
3. Current Land Uses: Private School Campus
4. Natural Features: None.
5. Adjacent Land Uses:
 - a) North: Residential (single family detached)
 - b) South: Residential (single family detached)
 - c) East (across N College Street): Residential (single family detached)
 - d) West: Recreation (sports field)
6. Zoning: The following zoning districts about the subject property.
 - a) North: R-2 / Medium Density Residential
 - b) East: R-1 / Low Density Residential
 - c) South: R-1 / Low Density Residential
 - d) West: R-1 / Low Density Residential

7. Access and Transportation: Access to the subject property is provided from N College Street. It is classified as a minor arterial under the jurisdiction of Oregon Department of Transportation (ODOT).
8. Utilities:
 - a) Water: There is an 8-inch water line located on N College Street with a 2-inch service lateral serving the property. Fire flow will need to be confirmed by a fire flow test.
 - b) Wastewater: There is a 21-inch wastewater line located on N College Street.
 - c) Stormwater: There is a 12-inch storm line located in E Pinehurst Drive with a 12-inch lateral serving the property.
 - d) Overhead Lines: Any new connection to the property will need to be undergrounded. See NMC 15.430.010 for exception provisions.

C. PROCESS:

The Site Design Review and Code Adjustment request is a Type II application and follows the procedures in NMC 15.100.030 and 15.100.220. Following a 15-day referral comment period, and a 14-day public comment period, the Community Development Director makes a decision on the application based on the criteria listed in the attached findings. The Director's decision is final unless appealed.

Important dates related to this application are as follows:

1. 04/11/2023: The Community Development Director deemed the application complete.
2. 04/19/2023: Notice for public comment was posted and mailed.
3. 04/24/2023: Application was routed for a 15-day referral review.
4. 07/18/2023: The Community Development Director issued a decision on the application.

No public comments were received during the public comment period occurring April 19, 2023, to May 3, 2023.

D. AGENCY COMMENTS:

The application was routed to several public agencies for review and comment (Attachment 2). Comments and recommendations from City departments have been incorporated into the findings and conditions. As of the writing of this report, the City received the following agency comments:

1. Building Official: Reviewed, provided comment stating:
All ADA (Americans with Disabilities Act) and code requirements will need to be met.
2. City Manager: Reviewed, no conflict.
3. Community Development Director: Reviewed, no conflict.
4. Finance Department: Reviewed. Comment provided stating:
The City has three liens on the property.

Details related to the liens were submitted along with comments and are provided in Attachment 2.
5. Police: Reviewed, no conflict.
6. Public Works Director: Reviewed, no conflict.
7. Public Works Maintenance Superintendent: Reviewed. Comments provided stating:
All building run off should go to the rain garden first before being discharged. How does any of the new or existing building accomplish this?
8. Public Works Water Superintendent: Reviewed, no conflict.
9. Public Works Waste Water Treatment Plant: Reviewed, no conflict.
10. Ziplly Fiber: Reviewed, no conflict.

E. ANALYSIS:

The subject property is a ±3.46-acre school that includes two primary buildings, gymnasium, portable classroom, covered play area, and 88 off-street parking stalls.

The proposed project will add a ±1,792 square-foot modular building to provide additional classroom space on the subject property along expansion of an existing rain water garden stormwater facility. Additionally, the proposed project is seeking a code adjustment to the minimum parking standards in order to maintain the existing off-street parking in its current quantity and configuration.

The subject property is located in the R-1 zoning district. Pursuant to NMC 15.305.020, primary and secondary schools are *Permitted* use in the R-1 zoning district.

The subject property is also located in the Inner Horizontal Surface of the Airport Overlay Zone (AO) associated with Sportsman Airpark. Pursuant to NMC 15.05.030, the Conical Surface is one of five of the airport's imaginary surfaces, and any object extending above

these imaginary surfaces is an obstruction. The definition of a “Conical Surface” is an area that begins at 150 feet above the airport elevation extending upward to a height of 350 feet. According to the facility details provided by the Federal Aviation Administration, the elevation of Sportsman Airpark is ± 181 feet. The subject property is at an elevation of ± 199 feet, meaning the Conical Surface begins ± 132 above the subject property. Pursuant to NMC 15.415.020(A)(1) and 15.415.020(A)(4), properties in the R-1 zoning district may not have a main building that exceeds 30 feet in height or an accessory building greater than 800 square feet that exceeds 16 feet in height. According to the application materials, the proposed modular building will be 14.5 feet tall.

Section II: Findings – File DR223-0002

C.S. Lewis Academy – New Modular Building Site Design Review – DR223-0002/ADJC 23-0004 – 1605 N College Street

A. FINDINGS FOR CODE ADJUSTMENTS (NMC CHAPTER 15.210)

Chapter 15.210 Code Adjustments

15.210.020 Type I adjustments and approval criteria.

The director may authorize adjustments from the following requirements through a Type I procedure subject to the following:

[...]

C. Dimensional Standards and Minimum Number of Off-Street Parking Spaces.

- 1. The director may approve adjustments to the dimensional standards of off-street parking spaces; standards for minimum number of off-street parking spaces; and required spaces to be used for compact cars excepting handicapped parking requirements.*

Finding: The application proposes to place a ±1,792-square-foot modular classroom on an existing school campus that includes K-12 instruction.

According to the application materials (Attachment 1), the school campus has 88 off-street parking spaces. Pursuant to NMC 15.440.030, high schools are required to provide 1.5 parking spaces per teaching station and eight spaces per classroom or one space per 42 square feet; elementary schools must provide 1.5 spaces per teaching station and four spaces per classroom or one space per 28 square feet.

The application materials indicate that based on the number of classrooms, the high school requires 38 parking spaces, and the elementary/middle school requires 66 spaces. The additional modular building will accommodate two new classrooms for middle/elementary school classes, requiring 11 off-street parking spaces, which brings the total number of required parking spaces to 115. The Applicant has indicated that based on the use of the site, there is no need for additional off-street parking.

Therefore, the adjustment requests to maintain the existing 88 spaces rather than expanding the parking area to include 115 parking spaces. Because the Applicant requests a reduction of 27 parking spaces from the minimum number requested, the request fulfills the requirement for an adjustment to minimum number of parking spaces.

The criterion is met.

- 2. Approval Criteria. The director shall find that approval will provide adequate off-street parking in relation to user demands. The following factors may be considered in granting an adjustment:*

- a. *Special characteristics of users which indicate low demand for off-street parking (e.g., low income, elderly).*
- b. *Opportunities for joint use of nearby off-street parking facilities.*
- c. *Availability of public transit.*
- d. *Natural features of the site (topography, vegetation and drainage) which would be adversely affected by application of required parking standards.*
- e. *Possible conversion of the site to other uses in the future.*
- f. *No adjustment shall be greater than 25 percent of the requirement from which the exception is granted*

[...]

Finding: The application proposes to place a ±1,792-square-foot modular classroom on an existing school campus that includes K-12 instruction.

The subject property is an existing school campus that has 88 off-street parking spaces.

The application materials indicate that based on the number of classrooms, the high school use requires 38 parking spaces, and the elementary/middle school use requires 66 spaces. The additional modular building will accommodate two new classrooms for middle/elementary school classes, requiring an additional 11 off-street parking spaces, which brings the total number of required parking spaces to 115.

Therefore, the adjustment requests to maintain the existing 88 spaces rather than expanding the parking area to include 115 parking spaces, which would result in a reduction of 27 parking spaces from the minimum number required pursuant to the NMC.

- a) **Special Characteristics:** This application involves placing a new structure on a private school campus. The school is primarily used by children under the legal age to drive, and the additional school classrooms will be used by elementary/middle school classes of student below the minimum legal age to drive. The school has limited bus service, meaning most students are picked up and dropped off by their primary caregiver in a personal vehicle. According to application materials, vehicle traffic is highest during drop-off and pick-up times (morning and afternoon), and cars typically queue in the travel aisles or only park temporarily.

The school possesses unique traffic and use patterns created by the school's limited access to buses which creates special characteristic that makes traffic management and space for vehicle queuing a priority over additional off-street parking spaces.

Because the age of students proposed to use the new modular classrooms will be less than the minimum legal age for driving, the additional off-street parking demand is anticipated to be minimal and may be used in consideration of granting an adjustment to the off-street parking requirements.

- b) **Opportunities for Joint Use Parking:** The C.S. Lewis Academy campus is within the R-1 zoning district. Single-family residences occupy surrounding properties with off-street parking spaces limited to individual driveways and garages. There are no areas

immediately adjacent to the site where the Applicant could establish a parking agreement for potential joint use.

Because there are no opportunities for joint use parking in areas abutting or near the subject property, this factor should not be considered in granting an adjustment to off-street parking requirements.

- c) **Availability of Public Transit:** According to the Yamhill County Transit Route Map, Routes 5 and 7 provide service to the City of Newberg. Bus Route 5 stops at the intersection of E Aldercrest Drive and N Meridian Street and the corner of E Crestview Drive and N College Street. Both stops are less than 1,000 feet from C.S. Lewis Academy. Therefore, public transit is available to the site.

Because the availability of public transit reduces the need for off-street parking on the subject property, this factor may be considered in granting an adjustment to off-street parking requirements.

- d) **Natural Features:** According to contour data on the Interactive Planning Map, the site is relatively flat, and no mapped stream corridors impact the subject property. The US Fish & Wildlife Service (USFWS) National Wetlands Inventory (NWI) indicates there are no wetlands on the site. The existing vegetation on the campus is primarily a grassy lawn used as a playground/sports field associated with the school.

Expansion of the existing off-street parking area is not limited by the natural features or topography of the subject property.

- e) **Possible Conversion of the Site to Other Uses:** The C.S. Lewis Academy campus is in the R-1 (Low-Density Residential) zoning district. A change of use in the existing buildings will require permitting, including an assessment of the available parking based on the new use.

Conversion of the existing buildings for future use should not be a consideration in granting an adjustment to the off-street parking requirements.

- f) **No Adjustment Greater than 25 Percent:** The adjustment requests to maintain the existing 88 spaces rather than expanding the parking area to include 115 parking spaces required pursuant to NMC , which would results in a reduction of 27 parking spaces from the minimum number required pursuant to the NMC 15.440.030. The requested adjustment will result in a 23.4 percent reduction to the off-street parking standard.

Because the proposed adjustment is less than 25 percent of the standard pursuant to NMC 15.440.030, this factor may be considered in granting an adjustment to off-street parking requirements.

The age of students proposed to use the new modular classrooms will be less than the minimum legal age for driving, public transit is available to the site, and the adjustment request is less than 25 percent of the standard from which the exception would be granted. Because the request appropriately considers factors related to special characteristics, availability of public transit, and the extent to which the standard would be modified, the criteria are met.

Conclusion: The criteria are met and the code adjustment for a reduction of 27 parking spaces should be granted.

B. FINDINGS FOR SITE DESIGN REVIEW (NMC CHAPTER 15.220)*Chapter 15.220 Site Design Review**15.220.050 Criteria for design review****B. Type II. The following criteria are required to be met in order to approve a Type II design review request:***

- 1. Design Compatibility. The proposed design review request incorporates an architectural design which is compatible with and/or superior to existing or proposed uses and structures in the surrounding area. This shall include, but not be limited to, building architecture, materials, colors, roof design, landscape design, and signage.***

Finding: As noted in the application materials (Attachment 1), the proposed building will have wood siding with asphalt shingle roofing similar to the neighboring single-family dwellings. The proposed addition is also similar in size and use to an existing portable classroom that is already on the subject property. Therefore, the design is compatible with the surrounding uses.

This criterion is met.

- 2. Parking and On-Site Circulation. Parking areas shall meet the requirements of NMC 15.440.010. Parking studies may be required to determine if adequate parking and circulation are provided for uses not specifically identified in NMC 15.440.010. Provisions shall be made to provide efficient and adequate on-site circulation without using the public streets as part of the parking lot circulation pattern. Parking areas shall be designed so that vehicles can efficiently enter and exit the public streets with a minimum impact on the functioning of the public street.***

Finding: The proposed project will install a ±1,792-square-foot modular classroom building on an existing school campus that houses a high school, middle school, and elementary school. The Applicant has included a request for an adjustment to the minimum off-street parking requirements as a part of this application. According to the application materials (Attachment 1), the site has 88 off-street parking spaces. The minimum number of required parking spaces is calculated based on the number of classrooms/teacher stations and the grade levels instructed and the subject property should provide 115 off-street parking spaces pursuant to NMC 15.440.030.

However, the application materials request a code adjustment reducing the minimum number of parking spaces to be provided by 27 parking spaces, or 23.4 percent. Because the proposal satisfies the adjustment criteria in 15.210.020(C), the proposed project will fulfill the requirements for parking and circulation.

This criterion is met.

- 3. Setbacks and General Requirements. The proposal shall comply with NMC 15.415.010 through 15.415.060 dealing with height restrictions and public access;***

and NMC 15.405.010 through 15.405.040 and 15.410.010 through 15.410.070 dealing with setbacks, coverage, vision clearance, and yard requirements.

Finding: This proposed project will install a new classroom building on a school campus. Because the site is located in the R-1 zoning district, a 25-foot setback from all property lines applies that exceeds the minimum standards for residential buildings pursuant to NMC 15.410.010.

According to the application materials (Attachment 1), the proposed building will be located at the center of the site, and the nearest property line will be ± 41 feet from the building. Because the proposed building will be sited more than 25 feet from any property line, the proposed project's configuration exceeds the minimum setback for the subject property.

This criterion is met.

4. *Landscaping Requirements. The proposal shall comply with NMC 15.420.010 dealing with landscape requirements and landscape screening.*

Finding: The subject property is ± 3.54 acres ($\pm 150,717$ square feet) in size and located in the R-1 zoning district.

Pursuant to NMC 15.420.010(B)(1), development in the R-1 zoning district is required to dedicate 15 percent of the site to landscaping. According to the application materials, the parking area and buildings (existing and proposed) occupy 1.77 acres ($\pm 77,312$ square feet) of the site. The remaining 1.76 acres ($\pm 76,665$ square feet), or 50.9 percent of the subject property, is landscaped. Because more than 15 percent of the subject property is landscaped, the proposed project will exceed the minimum required amount of landscaping.

This criterion is met.

5. *Signs. Signs shall comply with NMC 15.435.010 et seq. dealing with signs.*

Finding: An application for sign review was not submitted as part of this site design review. Because the proposed project does not include new or modified signage, this criterion is not applicable.

6. *Manufactured Dwelling, Mobile Home and RV Parks. Manufactured dwelling and mobile home parks shall also comply with the standards listed in NMC 15.445.075 through 15.445.100 in addition to the other clear and objective criteria listed in this section. RV parks also shall comply with NMC 15.445.170 in addition to the other criteria listed in this section.*

Finding: Because the application does not involve a manufactured dwelling, mobile home, or recreational vehicle (RV) park, this criterion is not applicable.

7. *Zoning District Compliance. The proposed use shall be listed as a permitted or conditionally permitted use in the zoning district in which it is located as found in NMC 15.305.010 through 15.336.020. Through this site review process, the director may make a determination that a use is determined to be similar to those listed in*

the applicable zoning district, if it is not already specifically listed. In this case, the director shall make a finding that the use shall not have any different or more detrimental effects upon the adjoining neighborhood area than those specifically listed.

Finding: The proposed project will install a new modular classroom building on the campus of an existing school C.S. Lewis Academy. The subject property is located in the R-1 zoning district.

According to the Zoning Use Table in NMC 15.305.020, primary and secondary schools are considered to be *Permitted* uses in the R-1 zoning district. Because the subject property is located in the R-1 zoning district and will continue its ongoing operations as a school, the use is *Permitted*.

This criterion is met.

8. *Subdistrict Compliance. Properties located within subdistricts shall comply with the provisions of those subdistricts located in NMC 15.340.010 through 15.348.060.*

Finding: The Newberg Interactive Map indicates that the subject property is located in the Inner Horizontal Surface of the Airport Overlay Zone (AO) associated with Sportsman Airpark.

Per NMC 15.05.030, the Conical Surface is one of five of the airport's imaginary surfaces, and any object extending above these imaginary surfaces is an obstruction. The definition of a Conical Surface is an area that begins at 150 feet above the airport elevation extending upward to a height of 350 feet. According to the facility details provided by the FAA, the elevation of Sportsman Airpark is ± 181 feet. According to Google Earth, the subject property is at an elevation of ± 199 feet, meaning the Conical Surface begins ± 132 above the subject property. Pursuant to NMC 15.415.020(A)(1) and (4), properties in the R-1 zoning district may not have a main building that exceeds 30 feet in height or an accessory building greater than 800 square feet that exceeds 16 feet in height. According to the application materials, the proposed modular building will be 14.5 feet tall. Because the proposed project will occur at a height and lower than the Conical Surface air space located above the subject property, the proposed project will not be an obstruction.

This criterion is met.

9. *Alternative Circulation, Roadway Frontage Improvements and Utility Improvements. Where applicable, new developments shall provide for access for vehicles and pedestrians to adjacent properties which are currently developed or will be developed in the future. This may be accomplished through the provision of local public streets or private access and utility easements. At the time of development of a parcel, provisions shall be made to develop the adjacent street frontage in accordance with city street standards and the standards contained in the transportation plan. At the discretion of the city, these improvements may be deferred through use of a deferred improvement agreement or other form of security.*

Finding: The subject property has frontage on N College Street. The portion of street frontage along the subject property is already improved and the proposed project will install a new ±17,092 square foot modular building on the site to provide additional classroom space. The parking area and on-site vehicle circulation will not be altered as a part of this project. Ingress and egress to the parking area are provided via two established driveway (one-way entrance and exit) connections to N College Street.

The portion of N College Street along the subject site's frontage was recently improved through Capital Improvement Project 707613/707528. Because the subject property's roadway frontage is already improved, no further improvements are necessary.

This criterion is met.

10. Traffic Study Improvements. If a traffic study is required, improvements identified in the traffic study shall be implemented as required by the director.

Finding: The proposed project will install a new ±1,792-square-foot modular building to provide additional classroom space to the C.S. Lewis Academy.

The Institute of Transportation Engineers (ITE), Trip Generation Manual, 10th Edition, Trip Code 536 Private School (K-12) cites the PM peak hour vehicle trip rate is 5.5 per 1,000 square feet of gross floor area. For the proposed modular addition of 1,792 square feet this equates to 9.9 pm peak hour vehicle trips. According to the application materials, the building will include two classrooms/teaching stations. Per the Institute of Traffic Engineers (ITE) Trip Generation Manual (11th Edition), the two classrooms/teaching stations will result in 18 a.m. peak hour trips and 10 p.m. peak hour trips.

Because the expected vehicle trips project is expected to generate less than 40 trips per p.m. peak hour, the traffic impact is not considered significant.

This criterion is not applicable.

C. FINDINGS FOR DEVELOPMENT STANDARDS (NMC DIVISION 15.400)

Chapter 15.405 Lot Requirements

[...] 15.405.020

B. Maximum Lot or Development Site Area per Dwelling Unit.

1. In the R-1 district, the average size of lots in a subdivision intended for single-family development shall not exceed 10,000 square feet.

[...]

C. In calculating lot area for this section, lot area does not include land within public or private streets. In calculating lot area for maximum lot area/minimum density

requirements, lot area does not include land within stream corridors, land reserved for public parks or open spaces, commons buildings, land for preservation of natural, scenic, or historic resources, land on slopes exceeding 15 percent or for avoidance of identified natural hazards, land in shared access easements, public walkways, or entirely used for utilities, land held in reserve in accordance with a future development plan, or land for uses not appurtenant to the residence.

D. Lot size averaging is allowed for any subdivision. Some lots may be under the minimum lot size required in the zone where the subdivision is located, as long as the average size of all lots is at least the minimum lot size.

Finding: The subject property is ±3.54 acres (±150,717 square feet) in size and located in the R-1 zoning district.

Because the subject site is an existing legal lot of record and the dimensions of the subject property will not be modified, the criterion is not applicable pursuant to NMC 15.405.020.

15.405.020 Lot area exceptions.

The following shall be exceptions to the required lot areas:

A. Lots of record with less than the area required by this code.

B. Lots or development sites which, as a process of their creation, were approved in accordance with this code.

C. Planned unit developments, provided they conform to requirements for planned unit development approval.

Finding: Because the subject site is an existing legal lot of record and the dimensions of the subject property will not be modified, the criterion is not applicable.

15.405.030 Lot dimensions and frontage.

[...]

Finding: Because the subject site is an existing legal lot of record and the dimensions of the subject property will not be modified, the criterion is not applicable.

15.405.040 Lot coverage and parking coverage requirements.

[...]

B. Residential uses in residential zones shall meet the following maximum lot coverage and parking coverage standards; however, cottage cluster projects shall be exempt from the standards. See the definitions in NMC 15.05.030 and Appendix A, Figure 4.

1. Maximum Lot Coverage.

a. R-1: 40 percent, except:

i. Fifty percent if all structures on the lot are one story; and

ii. Sixty percent for townhouse dwellings.

b. R-2 and RP: 60 percent.

c. AR and R-3: 60 percent.

2. Maximum Parking Coverage. R-1, R-2, R-3, and RP: 30 percent.

3. Combined Maximum Lot and Parking Coverage.

a. R-1: 60 percent.

b. R-2, R-3, RP and townhouse dwellings in R-1: 70 percent.

C. All other districts and uses not listed in subsection (B) of this section shall not be limited as to lot coverage and parking coverage except as otherwise required by this code.

Finding: The proposed project will install a new modular building to provide additional classroom space within an existing school campus. Pursuant to NMC 15.305.020, schools are not considered a residential use.

Because schools are not a use listed in NMC 15.405.040(B), lot and parking lot coverage limitations are not limited except as otherwise required by this code.

The criteria are met.

Chapter 15.410 Yard Setback Requirements

[...]

15.410.020 Front yard setback.

A. Residential (see Appendix A, Figure 10).

1. AR, R-1 and R-2 districts shall have a front yard of not less than 15 feet. Said yard shall be landscaped and maintained.

[...]

Finding: This project involves an existing school campus on a property in the R-1 zoning district. All abutting properties are also in a residential zoning district. Pursuant to NMC 15.410.040, schools must maintain a 25-foot setback from all property lines.

According to the application materials, the proposed building is located ± 41 from the nearest property line. Because the proposed structure will be more than 25 feet from the nearest property line, the proposed project exceeds the minimum requirement.

The criterion is met.

15.410.030 Interior yard setback.

A. Residential.

- 1. All lots or development sites in the AR, R-1, R-2 and R-3 districts shall have interior yards of not less than five feet, except that where a utility easement is recorded adjacent to a side lot line, there shall be a side yard no less than the width of the easement.*

[...]

Finding: This project involves an existing school campus on a property in the R-1 zoning district. All abutting properties are also in a residential zoning district. Pursuant to NMC 15.410.040, schools must maintain a 25-foot setback from all property lines.

According to the application materials, the proposed building is located ± 41 from the nearest property line. Because the proposed structure will be more than 25 feet from the nearest property line, the proposed project exceeds the minimum requirement.

The criterion is met.

15.410.040 Setback and yard restrictions as to schools, churches, public buildings.

- A. Building Setback. No buildings shall be erected, used or maintained for a school, church or public or semi-public building or use, institution or similar use under the regulations of this code unless such building is removed at least 25 feet from every boundary line of any property included in any residential district.*
- B. Required Yard. No required front or interior yard of the lot on which such building or use is located shall be used for play or parking purposes.*

Finding: This project involves an existing school campus on a property in the R-1 zoning district. All abutting properties are also in a residential zoning district. Pursuant to NMC 15.410.040, schools must maintain a 25-foot setback from all property lines.

According to the application materials, the proposed building is located ± 41 from the nearest property line. Because the proposed structure will be more than 25 feet from the nearest property line, the proposed project exceeds the minimum requirement.

The criterion is met.

[...]

15.410.060 Vision clearance setback.

The following vision clearance standards shall apply in all zones (see Appendix A, Figure 9).

[...]

- B. At the intersection of a private drive and a street, a triangle formed by the intersection of the curb lines, each leg of the vision clearance triangle shall be a minimum of 25 feet in length.*
- C. Vision clearance triangles shall be kept free of all visual obstructions from two and one-half feet to nine feet above the curb line. Where curbs are absent, the edge of the asphalt or future curb location shall be used as a guide, whichever provides the greatest amount of vision clearance.*

Finding: The subject site is improved with two established driveways. The southern driveway is an entrance only, and the northern driveway is an exit only. The exit-only driveway has a clear vision area free of visual obstructions. The Applicant is not proposing any alterations to the existing vision clearance areas.

The criterion is met.

Chapter 15.415 Building and Site Design Standards

[...]

15.415.020 Building height limitation.

A. Residential.

- 1. In the R-1 district, no main building shall exceed 30 feet in height, except that townhouse dwellings shall not exceed 35 feet in height.*

[...]

- 4. Accessory buildings in the R-1, R-2, R-3, AR, and RP districts are limited to 16 feet in height, except as follows:*
 - a. Up to 800 square feet of an accessory building may have a height of up to 24 feet.*
 - b. Aircraft hangars in the AR district may be the same height as the main building.*

Finding: The proposed project will install a new ±1,792-square-foot modular building to provide additional classroom space at an existing school campus and is considered an “accessory building.” Pursuant to NMC 15.05.030, an accessory building is considered any detached

subordinate building in which the use is incidental, appropriate, and subordinate to the main building.

According to the application materials, the proposed building will house additional classrooms separate from the primary school buildings. Because the proposed structure is an accessory structure and the requirements for 15.415.020(A)(4) apply. The Applicant's narrative indicates that the building will be ±14.5 feet tall.

Because the proposed project includes installation of an accessory structure that will be less than 16 feet in height, the proposed project will not exceed the maximum allowable height for accessory structures in the R-1 zoning district that are greater than 800 square feet in size.

The criteria are met.

[...]

15.415.040 Public access required.

No building or structure shall be erected or altered except on a lot fronting or abutting on a public street or having access to a public street over a private street or easement of record approved in accordance with provisions contained in this code. New private streets may not be created to provide access except as allowed under NMC 15.332.020(B)(24), 15.336.020(B)(8), and in the M-4 zone. Existing private streets may not be used for access for new dwelling units, except as allowed under NMC 15.405.030. No building or structure shall be erected or altered without provisions for access roadways as required in the Oregon Fire Code, as adopted by the city.

Finding: The subject property has established access to N College Street and the proposed project will not create any private streets.

The criteria are met.

Chapter 15.420 Landscaping and Outdoor Areas

15.420.010 Required minimum standards.

[...]

B. Required Landscaped Area. The following landscape requirements are established for all developments except single-family dwellings:

- 1. A minimum of 15 percent of the lot area shall be landscaped; provided, however, that computation of this minimum may include areas landscaped under subsection (B)(3) of this section. Development in the C-3 (central business district) zoning district and M-4 (large lot industrial) zoning district is exempt from the 15 percent landscape area requirement of this section. Additional landscaping requirements in the C-4 district are described in NMC 15.352.040(K). In the AI airport industrial district, only a five percent landscaping standard is required with the goal of "softening" the buildings and making the development "green" with plants, where***

possible. The existence of the runway, taxiway, and approach open areas already provide generally for the 15 percent requirement. Developments in the AI airport industrial district with a public street frontage shall have said minimum landscaping between the front property line and the front of the building.

[...]

Finding: The subject project property is ±3.54 acres (±150,717 square feet) in size and located in the R-1 zoning district. Development in the R-1 zoning district is required to provide a minimum of 15 percent of the site to landscaping.

According to the application materials, the parking area and buildings (existing and proposed) occupy 1.77 acres (±77,312 square feet) of the site. The application materials (Attachment 1) indicate that the remaining 1.76 acres (±76,665 square feet), or 50.9 percent of the property, is landscaped. Because more than 15 percent of the subject property includes landscaped areas, the proposed project and subject property exceed the minimum requirement for provision of landscaping.

The criteria are met.

15.420.020 Landscaping and amenities in public rights-of-way.

A. Pedestrian Space Landscaping. Pedestrian spaces shall include all sidewalks and medians used for pedestrian refuge. Spaces near sidewalks shall provide plant material for cooling and dust control, and street furniture for comfort and safety, such as benches, waste receptacles and pedestrian-scale lighting. These spaces should be designed for short-term as well as long-term use. Elements of pedestrian spaces shall not obstruct sightlines and shall adhere to any other required city safety measures. Medians used for pedestrian refuge shall be designed for short-term use only with plant material for cooling and dust control, and pedestrian-scale lighting. The design of these spaces shall facilitate safe pedestrian crossing with lighting and accent paving to delineate a safe crossing zone visually clear to motorists and pedestrians alike.

[...]

B. Planting Strip Landscaping. All planting strips shall be landscaped. Planting strips provide a physical and psychological buffer for pedestrians from traffic with plant material that reduces heat and dust, creating a more comfortable pedestrian environment. Planting strips shall have different arrangements and combinations of plant materials according to the frequency of on-street parking (see Appendix A, Figures 18 and 19).

[...]

C. Maintenance. All landscapes shall be maintained for the duration of the planting to encourage health of plant material as well as public health and safety. All street trees and shrubs shall be pruned to maintain health and structure of the plant material for public safety purposes.

[...]

Finding: The subject property has frontage along N College Street. N College Street was recently improved through Capital Improvement Project 707613/707528. Therefore, the Because the frontage of the subject property is already improved, the Applicant will not be required to complete frontage improvements and new street trees are not required.

The criterion is met.

Chapter 15.430 Underground Utility Installation

15.430.010 Underground utility installation.

- A. All new utility lines, including but not limited to electric, communication, natural gas, and cable television transmission lines, shall be placed underground. This does not include surface-mounted transformers, connections boxes, meter cabinets, service cabinets, temporary facilities during construction, and high-capacity electric lines operating at 50,000 volts or above.***
- B. Existing utility lines shall be placed underground when they are relocated, or when an addition or remodel requiring a Type II design review is proposed, or when a developed area is annexed to the city.***
- C. The director may make exceptions to the requirement to underground utilities based on one or more of the following criteria:***
 - 1. The cost of undergrounding the utility is extraordinarily expensive.***
 - 2. There are physical factors that make undergrounding extraordinarily difficult.***
 - 3. Existing utility facilities in the area are primarily overhead and are unlikely to be changed.***

Finding: The submitted plans do not show new connections to utilities. Because the proposed project will not create new utility connections, the criterion is not applicable.

Article I. Off-Street Parking Requirements

Chapter 15.440 Off-Street Parking, Bicycle Parking and Private Walkways

15.440.010 Required off-street parking

- A. Off-street parking shall be provided on the lot or development site for all R-1, C-1, M-1, M-2 and M-3 zones. In all other zones, the required parking shall be on the lot or development site or within 400 feet of the lot or development site which the parking is required to serve. All required parking must be under the same ownership as the lot or development site served except through special covenant agreements as approved by the city attorney, which bind the parking to the lot or development site.***
 - 1. In cases where the applicant is proposing off-street parking, refer to subsection (F) of this section for the maximum number of parking spaces.***

Finding: This proposal will add additional classroom space to an existing school campus. The Applicant is requesting an adjustment to provide less parking than the minimum required. Therefore, the maximum parking requirements in NMC 15.440.010(F) are not applicable.

15.440.030 *Parking spaces required.*

<i>Use</i>	<i>Minimum Parking Spaces Required</i>
<i>Schools</i>	<i>Elementary or junior high, 1-1/2 for each teaching station plus 4 for every classroom, or 1 for every 42 sq. ft. of seating area where there are no fixed seats in an auditorium or assembly area</i>
<i>Schools</i>	<i>High schools, 1-1/2 for each teaching station, plus 8 for every classroom, or 1 for every 28 sq. ft. of seating area where there are no fixed seats in an auditorium or assembly area</i>

Finding: This application proposes to place a new modular building on an existing school campus to provide additional classroom space.

NMC 15.440.030 determines the minimum parking requirements for schools based on the level of education. According to the application materials (Attachment 1), the additional classrooms are intended to serve elementary and middle school classes. With the new classrooms, the school is expected to have 18 classrooms/teaching stations in total. Four classrooms are dedicated to high school instruction, and 14 are devoted to elementary and middle school instruction, and a total of 115 off-street parking spaces are therefore required. The current parking area provides 88 parking spaces.

The application materials request a code adjustment to maintain the existing spaces without providing any additional parking, which is commensurate with reduction of 27 parking spaces.

See findings for Code Adjustment related to NMC 15.210.020.

Article II. Bicycle Parking

[...]

15.440.100 *Facility requirements.*

Bicycle parking facilities shall be provided for the uses shown in the following table. Fractional space requirements shall be rounded up to the next whole number.

<i>Use</i>	<i>Minimum Number of Bicycle Parking Spaces Required</i>
<i>New commercial, industrial, office, and institutional developments, including additions that total 4,000 square feet or more</i>	<i>One bicycle parking space for every 10,000 square feet of gross floor area. In C-4 districts, two bicycle parking spaces, or one per 5,000 square feet of building area, must be provided, whichever is greater</i>

Finding: This proposal involves placing a new building on a school campus. Per the application materials (Attachment 1), the proposed building is a 1,792-square-foot modular building, which does not exceed the 4,00 square foot threshold defined in NMC 15.440.100.

Because the proposed project will add less than 4,000 square feet, additional bicycle parking is not required.

The criterion is not applicable.

Article III. Private Walkways

[...]

15.440.130 Where required.

Private walkways shall be constructed as part of any development requiring Type II design review, including mobile home parks. In addition, they may be required as part of conditional use permits or planned unit developments. In the airport industrial (AI) district and residential (AR) district, on-site walks are not required in aircraft operations areas, such as parking aprons, taxiways, and runways.

15.440.140 Private walkway design.

- A. All required private walkways shall meet the applicable building code and Americans with Disabilities Act requirements.*
- B. Required private walkways shall be a minimum of four feet wide.*
- C. Required private walkways shall be constructed of portland cement concrete or brick.*
- D. Crosswalks crossing service drives shall, at a minimum, be painted on the asphalt or clearly marked with contrasting paving materials or humps/raised crossings. If painted*

striping is used, it should consist of thermoplastic striping or similar type of durable application.

- E. At a minimum, required private walkways shall connect each main pedestrian building entrance to each abutting public street and to each other.*
- F. The review body may require on-site walks to connect to development on adjoining sites.*
- G. The review body may modify these requirements where, in its opinion, the development provides adequate on-site pedestrian circulation, or where lot dimensions, existing building layout, or topography preclude compliance with these standards.*

Finding: According to the provided application materials (Site Plan), a five-foot-wide private walkway will give access to the proposed modular building. The subject property is improved with an existing private walkway that allows pedestrian circulation throughout the site. An access ramp will provide access from to a platform shared by the existing and proposed modular classrooms.

Because the application materials do not depict or describe how the existing private pedestrian pathway will provide a connection to the modular classroom's access ramp which is compliant with the Americans with Disabilities Act Requirements, the Applicant shall be required to provide a connection that meets the applicable building code and Americans with Disabilities Act requirements for access from the existing pedestrian pathway to the proposed modular classroom during the building permit process and prior to occupancy.

The criteria will be met if the aforementioned criterion is adhered to.

D. FINDINGS FOR PUBLIC IMPROVEMENT STANDARDS (NMC CHAPTER 15.505)

Chapter 15.505 Public Improvement Standards

15.505 Public Improvements Standards

[...]

15.505.020 Applicability.

The provision and utilization of public facilities and services within the City of Newberg shall apply to all land developments in accordance with this chapter. No development shall be approved unless the following improvements are provided for prior to occupancy or operation, unless future provision is assured in accordance with NMC 15.505.030(E).

- A. Public Works Design and Construction Standards. The design and construction of all improvements within existing and proposed rights-of-way and easements, all improvements to be maintained by the city, and all improvements for which city approval is required shall comply with the requirements of the most recently adopted Newberg public works design and construction standards.*

Finding: All improvements reviewed under this application are identified in the NMC 15.505 section specific to them and are conditioned to comply with the Public Works Design and Construction Standards in those sections.

This criterion is met.

B. Street Improvements. *All projects subject to a Type II design review, partition, or subdivision approval must construct street improvements necessary to serve the development.*

Finding: N College Street adjacent to the proposed development is already improved.

This criterion is met.

C. Water. *All developments, lots, and parcels within the City of Newberg shall be served by the municipal water system as specified in Chapter 13.15 NMC.*

Finding: No new connection to the public water line is proposed. There is a hydrant at the entrance to the site. The Applicant submitted a fire flow test performed by Wyatt Fire Protect Inc. The fire flow test results performed by Wyatt Fire Protect Inc. shall be submitted with the building permit application to be reviewed by the Fire Marshall for approval.

This criterion will be met when the Fire Marshall determines flows are adequate for fire protection requirements.

D. Wastewater. *All developments, lots, and parcels within the City of Newberg shall be served by the municipal wastewater system as specified in Chapter 13.10 NMC.*

Finding: No new connection to the public wastewater line is proposed. The site is connected to the public wastewater line currently.

This criterion is met.

E. Stormwater. *All developments, lots, and parcels within the City of Newberg shall manage stormwater runoff as specified in Chapters 13.20 and 13.25 NMC.*

Finding: The proposed development will create more than 500 square feet of impervious area. The Applicant has submitted a LIDA sizing form to determine the size of facility needed to treat and detain the 1,792 square feet of new impervious area. The Applicant plans to increase the size of an existing facility to accommodate the new run off. The existing facility drains to the 12-inch private storm line which drains to a catch basin that connects to a public storm line in Pinehurst Drive.

This criterion is met.

F. Utility Easements. *Utility easements shall be provided as necessary and required by the review body to provide needed facilities for present or future development of the area.*

Finding: N College Street is under the jurisdiction of the Oregon Department of Transportation (ODOT). There is a public utility easement benefitting ODOT along the property frontage.

This criterion is met.

G. City Approval of Public Improvements Required. No building permit may be issued until all required public facility improvements are in place and approved by the director, or are otherwise bonded for in a manner approved by the review authority, in conformance with the provisions of this code and the Newberg Public Works Design and Construction Standards.

Finding: Any required public improvement permit(s) for this project must be submitted, approved, and issued prior to building permits being issued.

This criterion will be met if the aforementioned condition of approval is adhered to.

15.505.030 Street standards.

[...]

A. Purpose. The purpose of this section is to:

- 1. Provide for safe, efficient, and convenient multi-modal transportation within the City of Newberg.**
- 2. Provide adequate access to all proposed and anticipated developments in the City of Newberg. For purposes of this section, “adequate access” means direct routes of travel between destinations; such destinations may include residential neighborhoods, parks, schools, shopping areas, and employment centers.**
- 3. Provide adequate area in all public rights-of-way for sidewalks, wastewater and water lines, stormwater facilities, natural gas lines, power lines, and other utilities commonly and appropriately placed in such rights-of-way. For purposes of this section, “adequate area” means space sufficient to provide all required public services to standards defined in this code and in the Newberg public works design and construction standards.**

B. Applicability. The provisions of this section apply to:

- 1. The creation, dedication, and/or construction of all public streets, bike facilities, or pedestrian facilities in all subdivisions, partitions, or other developments in the City of Newberg.**
- 2. The extension or widening of existing public street rights-of-way, easements, or street improvements including those which may be proposed by an individual or the city, or which may be required by the city in association with other development approvals.**

3. The construction or modification of any utilities, pedestrian facilities, or bike facilities in public rights-of-way or easements.

4. The designation of planter strips. Street trees are required subject to Chapter 15.420 NMC.

5. Developments outside the city that tie into or take access from city streets.

C. Layout of Streets, Alleys, Bikeways, and Walkways. *Streets, alleys, bikeways, and walkways shall be laid out and constructed as shown in the Newberg transportation system plan. In areas where the transportation system plan or future street plans do not show specific transportation improvements, roads and streets shall be laid out so as to conform to previously approved subdivisions, partitions, and other developments for adjoining properties, unless it is found in the public interest to modify these patterns. Transportation improvements shall conform to the standards within the Newberg Municipal Code, the Newberg public works design and construction standards, the Newberg transportation system plan, and other adopted city plans.*

D. Construction of New Streets. *Where new streets are necessary to serve a new development, subdivision, or partition, right-of-way dedication and full street improvements shall be required. Three-quarter streets may be approved in lieu of full street improvements when the city finds it to be practical to require the completion of the other one-quarter street improvement when the adjoining property is developed; in such cases, three-quarter street improvements may be allowed by the city only where all of the following criteria are met:*

1. The land abutting the opposite side of the new street is undeveloped and not part of the new development; and

2. The adjoining land abutting the opposite side of the street is within the city limits and the urban growth boundary.

Finding: Because N College Street is improved adjacent to the subject property, the requirements of sub-sections A, B, C, and D are met.

This criteria are met.

E. Improvements to Existing Streets.

1. All projects subject to partition, subdivision, or Type II design review approval shall dedicate right-of-way sufficient to improve the street to the width specified in subsection (G) of this section.

2. All projects subject to partition, subdivision, or Type II design review approval must construct a minimum of a three-quarter street improvement to all existing streets adjacent to, within, or necessary to serve the development. The director may waive or modify this requirement where the applicant demonstrates that the condition of existing streets to serve the development meets city standards and is in satisfactory condition to handle the projected

traffic loads from the development. Where a development has frontage on both sides of an existing street, full street improvements are required.

3. In lieu of the street improvement requirements outlined in NMC 15.505.040(B), the review authority may elect to accept from the applicant monies to be placed in a fund dedicated to the future reconstruction of the subject street(s). The amount of money deposited with the city shall be 100 percent of the estimated cost of the required street improvements (including any associated utility improvements), and 10 percent of the estimated cost for inflation. Cost estimates used for this purpose shall be based on preliminary design of the constructed street provided by the applicant's engineer and shall be approved by the director.

Finding: Regarding improvements to existing streets:

- 1) Because the right-of-way adjacent to the property along N College Street is sufficient, this criterion is met.
- 2) Because N College Street adjacent to the subject property is improved, this criterion is not applicable.
- 3) Because no street improvements are proposed or required, no monies are required in lieu of street improvements. This criterion is met.

The criteria are met.

F. Improvements Relating to Impacts. Improvements required as a condition of development approval shall be roughly proportional to the impact of the development on public facilities and services. The review body must make findings in the development approval that indicate how the required improvements are roughly proportional to the impact. Development may not occur until required transportation facilities are in place or guaranteed, in conformance with the provisions of this code. If required transportation facilities cannot be put in place or be guaranteed, then the review body shall deny the requested land use application.

Finding: Because no street improvements are proposed or required, this criterion is not applicable.

G. Street Width and Design Standards.

1. Design Standards. All streets shall conform with the standards contained in Table 15.505.030(G). Where a range of values is listed, the director shall determine the width based on a consideration of the total street section width needed, existing street widths, and existing development patterns. Preference shall be given to the higher value. Where values may be modified by the director, the overall width shall be determined using the standards under subsections (G)(2) through (10) of this section.

Table 15.505.030(G) Street Design Standards

Type of Street	Right-of-Way Width	Curb-to-Curb Pavement Width	Motor Vehicle Travel Lanes	Median Type	Striped Bike Lane (Both Sides)	On-Street Parking
Arterial Streets						
Expressway**	ODOT	ODOT	ODOT	ODOT	ODOT	ODOT
Major arterial	95 – 100 feet	74 feet	4 lanes	TWLTL or median*	Yes	No*
Minor arterial	69 – 80 feet	48 feet	2 lanes	TWLTL or median*	Yes	No*
Collectors						
Major	57 – 80 feet	36 feet	2 lanes	None*	Yes	No*
Minor	61 – 65 feet	40 feet	2 lanes	None*	Yes*	Yes*
Local Streets						
Local residential	54 – 60 feet	32 feet	2 lanes	None	No	Yes
Limited residential, parking both sides	44 – 50 feet	28 feet	2 lanes	None	No	Yes
Limited residential, parking one side	40 – 46 feet	26 feet	2 lanes	None	No	One side
Local commercial/ industrial	55 – 65 feet	34 feet	2 lanes	None*	No*	Yes*

* May be modified with approval of the director. Modification will change overall curb-to-curb and right-of-way width. Where a center turn lane is not required, a landscaped median shall be provided instead, with turning pockets as necessary to preserve roadway functions.

** All standards shall be per ODOT expressway standards.

Finding: Because no street improvements are proposed or required, this criterion is not applicable.

2. Motor Vehicle Travel Lanes. Collector and arterial streets shall have a minimum width of 12 feet.

a. Exception.

i. Minimum lane width of 11 feet along S River Street from E First Street to E Fourteenth Street.

Finding: Because no new collector or arterial streets are proposed, this criterion is not applicable.

3. Bike Lanes. Striped bike lanes shall be a minimum of six feet wide. Bike lanes shall be provided where shown in the Newberg transportation system plan.

a. Exception.

i. Minimum striped bike lane width of six feet with a one-foot wide buffer along S River Street from E First Street to the bypass.

Finding: Because no bike lanes are proposed or required, this criterion is not applicable.

4. Parking Lanes. *Where on-street parking is allowed on collector and arterial streets, the parking lane shall be a minimum of eight feet wide.*

a. Exception.

i. Minimum parking lane width of seven feet along S River Street from the bypass to E Fourteenth Street.

Finding: Because there are no collector or arterial streets proposed, this criterion is not applicable.

5. Center Turn Lanes. *Where a center turn lane is provided, it shall be a minimum of 12 feet wide.*

Finding: Because no center turn lanes are proposed, this criterion is not applicable.

6. Limited Residential Streets. *Limited residential streets shall be allowed only at the discretion of the review authority, and only in consideration of the following factors:*

a. The requirements of the fire chief shall be followed.

b. The estimated traffic volume on the street is low, and in no case more than 600 average daily trips.

c. Use for through streets or looped streets is preferred over cul-de-sac streets.

d. Use for short blocks (under 400 feet) is preferred over longer blocks.

e. The total number of residences or other uses accessing the street in that block is small, and in no case more than 30 residences.

f. On-street parking usage is limited, such as by providing ample off-street parking, or by staggering driveways so there are few areas where parking is allowable on both sides.

Finding: Because limited residential streets are not proposed or required, this criterion is not applicable.

7. Sidewalks. *Sidewalks shall be provided on both sides of all public streets. Minimum width is five feet.*

a. Exception.

i. Twelve-foot-wide sidewalks, inclusive of the curb, with tree wells along S River Street from the bypass to E Fourteenth Street.

ii. Twelve-foot-wide shared-use path and four-foot buffer, inclusive of the curb, with tree wells along the east side of S River Street from the bypass to E Fourteenth Street.

Finding: Because sidewalks on N College Street are already improved, this criterion is met.

8. Planter Strips. Except where infeasible, a planter strip shall be provided between the sidewalk and the curb line, with a minimum width of five feet. This strip shall be landscaped in accordance with the standards in NMC 15.420.020. Curb-side sidewalks may be allowed on limited residential streets. Where curb-side sidewalks are allowed, the following shall be provided:

a. Additional reinforcement is done to the sidewalk section at corners.

b. Sidewalk width is six feet.

Finding: The existing sidewalks on N College Street are curb tight, no planter strips were included in the existing street improvements completed previously. Because the frontage is already improved with curb tight sidewalks, this criterion is not applicable.

9. Slope Easements. Slope easements shall be provided adjacent to the street where required to maintain the stability of the street.

Finding: The Applicant is not proposing a slope easement, and none is required.

This criterion is not applicable.

10. Intersections and Street Design. The street design standards in the Newberg public works design and construction standards shall apply to all public streets, alleys, bike facilities, and sidewalks in the city.

Finding: Because the Applicant is not proposing street improvements and none are required, this criterion is not applicable.

11. The planning commission may approve modifications to street standards for the purpose of ingress or egress to a minimum of three and a maximum of six lots through a conditional use permit.

Finding: The Applicant is not proposing modifications to street standards for the purpose of ingress or egress.

This criterion is not applicable.

H. Modification of Street Right-of-Way and Improvement Width. The director, pursuant to the Type II review procedures of Chapter 15.220 NMC, may allow modification to the public street standards of subsection (G) of this section, when the criteria in both subsections (H)(1) and (2) of this section are satisfied:

1. The modification is necessary to provide design flexibility in instances where:

- a. Unusual topographic conditions require a reduced width or grade separation of improved surfaces; or*
- b. Lot shape or configuration precludes accessing a proposed development with a street which meets the full standards of this section; or*
- c. A modification is necessary to preserve trees or other natural features determined by the city to be significant to the aesthetic character of the area; or*
- d. A planned unit development is proposed and the modification of street standards is necessary to provide greater privacy or aesthetic quality to the development.*

2. Modification of the standards of this section shall only be approved if the director finds that the specific design proposed provides adequate vehicular access based on anticipated traffic volumes.

Finding: Because the Applicant has not proposed modifications to these street standards, the criteria are not applicable.

I. Temporary Turnarounds. Where a street will be extended as part of a future phase of a development, or as part of development of an abutting property, the street may be terminated with a temporary turnaround in lieu of a standard street connection or circular cul-de-sac bulb. The director and fire chief shall approve the temporary turnaround. It shall have an all-weather surface, and may include a hammerhead-type turnaround meeting fire apparatus access road standards, a paved or graveled circular turnaround, or a paved or graveled temporary access road. For streets extending less than 150 feet and/or with no significant access, the director may approve the street without a temporary turnaround. Easements or right-of-way may be required as necessary to preserve access to the turnaround.

Finding: Because the Applicant is not proposing a temporary turnaround, this criterion is not applicable.

J. Topography. The layout of streets shall give suitable recognition to surrounding topographical conditions in accordance with the purpose of this code.

Finding: Because the Applicant is not proposing street improvements, this criterion is not applicable.

K. Future Extension of Streets. All new streets required for a subdivision, partition, or a project requiring site design review shall be constructed to be “to and through”:
through the development and to the edges of the project site to serve adjacent properties for future development.

Finding: Because there are no future street extensions proposed as part of this project, this criterion is not applicable.

L. Cul-de-Sacs.

1. Cul-de-sacs shall only be permitted when one or more of the circumstances listed in this section exist. When cul-de-sacs are justified, public walkway connections shall be provided wherever practical to connect with another street, walkway, school, or similar destination.

a. Physical or topographic conditions make a street connection impracticable. These conditions include but are not limited to controlled access streets, railroads, steep slopes, wetlands, or water bodies where a connection could not be reasonably made.

b. Buildings or other existing development on adjacent lands physically preclude a connection now or in the future, considering the potential for redevelopment.

c. Where streets or accessways would violate provisions of leases, easements, or similar restrictions.

d. Where the streets or accessways abut the urban growth boundary and rural resource land in farm or forest use, except where the adjoining land is designated as an urban reserve area.

2. Cul-de-sacs shall be no more than 400 feet long (measured from the centerline of the intersection to the radius point of the bulb).

3. Cul-de-sacs shall not serve more than 18 single-family dwellings.

Each cul-de-sac shall have a circular end with a minimum diameter of 96 feet, curb-to-curb, within a 109-foot minimum diameter right-of-way. For residential uses, a 35-foot radius may be allowed if the street has no parking, a mountable curb, curbside sidewalks, and sprinkler systems in every building along the street.

Finding: Because the Applicant is not proposing a cul-de-sac, this criterion is not applicable.

M. Street Names and Street Signs. Streets that are in alignment with existing named streets shall bear the names of such existing streets. Names for new streets not in alignment with existing streets are subject to approval by the director and the fire chief and shall not unnecessarily duplicate or resemble the name of any existing or platted street in the city. It shall be the responsibility of the land divider to provide street signs.

Finding: Because the proposed project is not naming streets or street signs, this criterion is not applicable.

N. Platting Standards for Alleys.

1. An alley may be required to be dedicated and constructed to provide adequate access for a development, as deemed necessary by the director.

2. *The right-of-way width and paving design for alleys shall be not less than 20 feet wide. Slope easements shall be dedicated in accordance with specifications adopted by the city council under NMC 15.505.010 et seq.*
3. *Where two alleys intersect, 10-foot corner cut-offs shall be provided.*
4. *Unless otherwise approved by the city engineer where topographical conditions will not reasonably permit, grades shall not exceed 12 percent on alleys, and centerline radii on curves shall be not less than 100 feet.*
5. *All provisions and requirements with respect to streets identified in this code shall apply to alleys the same in all respects as if the word “street” or “streets” therein appeared as the word “alley” or “alleys” respectively.*

Finding: Because the project is not proposing alleys, this criterion is not applicable.

O. Platting Standards for Blocks.

1. Purpose. Streets and walkways can provide convenient travel within a neighborhood and can serve to connect people and land uses. Large, uninterrupted blocks can serve as a barrier to travel, especially walking and biking. Large blocks also can divide rather than unite neighborhoods. To promote connected neighborhoods and to shorten travel distances, the following minimum standards for block lengths are established.

2. Maximum Block Length and Perimeter. The maximum length and perimeters of blocks in the zones listed below shall be according to the following table. The review body for a subdivision, partition, conditional use permit, or a Type II design review may require installation of streets or walkways as necessary to meet the standards below.

<i>Zone(s)</i>	<i>Maximum Block Length</i>	<i>Maximum Block Perimeter</i>
<i>R-1</i>	<i>800 feet</i>	<i>2,000 feet</i>
<i>R-2, R-3, RP, I</i>	<i>1,200 feet</i>	<i>3,000 feet</i>

3. Exceptions.

a. If a public walkway is installed mid-block, the maximum block length and perimeter may be increased by 25 percent.

b. Where a proposed street divides a block, one of the resulting blocks may exceed the maximum block length and perimeter standards provided the average block length and perimeter of the two resulting blocks do not exceed these standards.

c. Blocks in excess of the above standards are allowed where access controlled streets, street access spacing standards, railroads, steep slopes, wetlands, water bodies, preexisting development, ownership patterns or similar circumstances restrict street and walkway location and design. In these cases, block length and perimeter shall be as small as practical. Where a street cannot be provided because of these circumstances but a public walkway is still feasible, a public walkway shall be provided.

d. Institutional campuses located in an R-1 zone may apply the standards for the institutional zone.

e. Where a block is in more than one zone, the standards of the majority of land in the proposed block shall apply.

f. Where a local street plan, concept master site development plan, or specific plan has been approved for an area, the block standards shall follow those approved in the plan. In approving such a plan, the review body shall follow the block standards listed above to the extent appropriate for the plan area.

4. Public Pedestrian Walkways and Bicycle Access. *The approval authority in approving a land use application with conditions may require a developer to provide an access way where the creation of a street consistent with street spacing standards is infeasible and the creation of a cul-de-sac or dead-end street is unavoidable. A public walkway provides a connection through a block that is longer than established standards or connects the end of the street to another right-of-way or a public access easement. A public walkway shall be contained within a public right-of-way or public access easement, as required by the city. A public walkway shall be a minimum of 10 feet wide and shall provide a minimum six-foot-wide paved surface or other all-weather surface approved by the city (see subsection (S) of this section for public walkway standards).*

Design features should be considered that allow access to emergency vehicles but that restrict access to non-emergency motorized vehicles.

Finding: Because the proposed project does not include the creation of new blocks or modification of existing blocks, this criterion is not applicable.

P. Private Streets. *New private streets, as defined in NMC 15.05.030, shall not be created, except as allowed by NMC 15.240.020(L)(2).*

Finding: Because the proposed project is not proposing private streets, this criterion is not applicable.

Q. Traffic Calming.

1. The following roadway design features may be required in new street construction where traffic calming needs are anticipated:

a. Serpentine alignment.

- b. Curb extensions.*
- c. Traffic diverters/circles.*
- d. Raised medians and landscaping.*
- e. Other methods shown effective through engineering studies.*

2. Traffic-calming measures such as speed humps should be applied to mitigate traffic operations and/or safety problems on existing streets. They should not be applied with new street constructions.

Finding: Because the proposed project does not propose traffic calming and none is required, this criterion is not applicable.

R. Vehicular Access Standards.

1. Purpose. *The purpose of these standards is to manage vehicle access to maintain traffic flow, safety, roadway capacity, and efficiency. They help to maintain an adequate level of service consistent with the functional classification of the street. Major roadways, including arterials and collectors, serve as the primary system for moving people and goods within and through the city. Access is limited and managed on these roads to promote efficient through movement. Local streets and alleys provide access to individual properties. Access is managed on these roads to maintain safe maneuvering of vehicles in and out of properties and to allow safe through movements. If vehicular access and circulation are not properly designed, these roadways will be unable to accommodate the needs of development and serve their transportation function.*

2. Access Spacing Standards. *Public street intersection and driveway spacing shall follow the standards in Table 15.505.R below. The Oregon Department of Transportation (ODOT) has jurisdiction of some roadways within the Newberg city limits, and ODOT access standards will apply on those roadways.*

Table 15.505.R. Access Spacing Standards

Roadway Functional Classification	Area ¹	Minimum Public Street Intersection Spacing (Feet) ²	Driveway Setback from Intersecting Street ³
Expressway	All	Refer to ODOT Access Spacing Standards	NA
Major arterial	Urban CBD	Refer to ODOT Access Spacing Standards	
Minor arterial	Urban CBD	500 200	150 100
Major collector	All	400	150
Minor collector	All	300	100

¹ "Urban" refers to intersections inside the city urban growth boundary outside the central business district (C-3 zone).

"CBD" refers to intersections within the central business district (C-3 zone).

Roadway Functional Classification	Area ¹	Minimum Public Street Intersection Spacing (Feet) ²	Driveway Setback from Intersecting Street ³
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“All” refers to all intersections within the Newberg urban growth boundary.

² *Measured centerline to centerline.*

³ *The setback is based on the higher classification of the intersecting streets. Measured from the curb line of the intersecting street to the beginning of the driveway, excluding flares. If the driveway setback listed above would preclude a lot from having at least one driveway, including shared driveways or driveways on adjoining streets, one driveway is allowed as far from the intersection as possible.*

Finding: Because the Applicant is not proposing a new access, this criterion is not applicable.

3. Properties with Multiple Frontages. *Where a property has frontage on more than one street, access shall be limited to the street with the lesser classification.*

a. For a duplex, triplex or quadplex dwelling or a cottage cluster project with frontage on two local streets, access may be permitted on both streets.

Finding: Because the subject property does not have multiple frontages and access will remain as it is currently, this criterion is not applicable.

4. Driveways. *More than one driveway is permitted on a lot accessed from either a minor collector or local street as long as there is at least 40 feet of lot frontage separating each driveway approach. More than one driveway is permitted on a lot accessed from a major collector as long as there is at least 100 feet of lot frontage separating each driveway approach.*

a. For a duplex, triplex or quadplex dwelling or a cottage cluster project, more than one driveway is permitted on a lot accessed from either a minor collector or local street as long as there is at least 22 feet of lot frontage separating each driveway approach.

Finding: The subject property has two driveways that have more than 40 feet between them.

Because the two on-site driveways have more than 40 feet between them, this criterion is met.

5. Alley Access. *Where a property has frontage on an alley and the only other frontages are on collector or arterial streets, access shall be taken from the alley only. The review body may allow creation of an alley for access to lots that do not otherwise have frontage on a public street provided all of the following are met:*

a. The review body finds that creating a public street frontage is not feasible.

b. The alley access is for no more than six dwellings and no more than six lots.

c. The alley has through access to streets on both ends.

d. One additional parking space over those otherwise required is provided for each dwelling. Where feasible, this shall be provided as a public use parking space adjacent to the alley.

Finding: Because the subject property does not have alley access, this criterion is not applicable.

6. Closure of Existing Accesses. *Existing accesses that are not used as part of development or redevelopment of a property shall be closed and replaced with curbing, sidewalks, and landscaping, as appropriate.*

Finding: Because the existing access is currently used and is not proposed for closure as part of this project, this criterion is not applicable.

7. Shared Driveways.

a. The number of driveways onto arterial streets shall be minimized by the use of shared driveways with adjoining lots where feasible. The city shall require shared driveways as a condition of land division or site design review, as applicable, for traffic safety and access management purposes. Where there is an abutting developable property, a shared driveway shall be provided as appropriate. When shared driveways are required, they shall be stubbed to adjacent developable parcels to indicate future extension. "Stub" means that a driveway temporarily ends at the property line, but may be accessed or extended in the future as the adjacent parcel develops. "Developable" means that a parcel is either vacant or it is likely to receive additional development (i.e., due to infill or redevelopment potential).

b. Access easements (i.e., for the benefit of affected properties) and maintenance agreements shall be recorded for all shared driveways, including pathways, at the time of final plat approval or as a condition of site development approval.

c. No more than four lots may access one shared driveway, with the exception of cottage dwellings on individual lots that are part of a cottage cluster.

d. Shared driveways shall be posted as no parking fire lanes where required by the fire marshal.

e. Where three or more lots share one driveway, one additional parking space over those otherwise required shall be provided for each dwelling. Where feasible, this shall be provided as a common use parking space adjacent to the driveway. However, duplex, triplex, quadplex, townhouse and cottage dwellings with shared driveways shall be exempt from this standard.

Finding: Because there is no shared driveway on the subject property and one is not proposed as part of this project, this criterion is not applicable.

8. Frontage Streets and Alleys. *The review body for a partition, subdivision, or design review may require construction of a frontage street to provide access to properties fronting an arterial or collector street.*

Finding: No frontage street is proposed or required.

This criterion is not applicable.

9. ODOT or Yamhill County Right-of-Way. *Where a property abuts an ODOT or Yamhill County right-of-way, the applicant for any development project shall obtain an access permit from ODOT or Yamhill County.*

Finding: Because the existing access is already permitted and will not be modified as part of the proposed project, this criterion is met.

10. Exceptions. *The director may allow exceptions to the access standards above in any of the following circumstances:*

a. Where existing and planned future development patterns or physical constraints, such as topography, parcel configuration, and similar conditions, prevent access in accordance with the above standards.

b. Where the proposal is to relocate an existing access for existing development, where the relocated access is closer to conformance with the standards above and does not increase the type or volume of access.

c. Where the proposed access results in safer access, less congestion, a better level of service, and more functional circulation, both on street and on site, than access otherwise allowed under these standards.

Finding: Because the proposed project is not requesting an exception, this criterion is not applicable.

11. *Where an exception is approved, the access shall be as safe and functional as practical in the particular circumstance. The director may require that the applicant submit a traffic study by a registered engineer to show the proposed access meets these criteria.*

Finding: Because the proposed project is not requesting an exception, this criterion is not applicable.

S. Public Walkways.

1. *Projects subject to Type II design review, partition, or subdivision approval may be required to provide public walkways where necessary for public safety and convenience, or where necessary to meet the standards of this code. Public walkways are meant to connect cul-de-sacs to adjacent areas, to pass through oddly shaped or unusually long blocks, to provide for networks of public paths according to adopted plans, or to provide access to schools, parks or other community destinations or public areas. Where practical, public walkway easements and locations may also be used to accommodate public utilities.*

2. *Public walkways shall be located within a public access easement that is a minimum of 15 feet in width.*

3. A walk strip, not less than 10 feet in width, shall be paved in the center of all public walkway easements. Such paving shall conform to specifications in the Newberg public works design and construction standards.

4. Public walkways shall be designed to meet the Americans with Disabilities Act requirements.

5. Public walkways connecting one right-of-way to another shall be designed to provide as short and straight of a route as practical.

6. The developer of the public walkway may be required to provide a homeowners' association or similar entity to maintain the public walkway and associated improvements.

7. Lighting may be required for public walkways in excess of 250 feet in length.

8. The review body may modify these requirements where it finds that topographic, preexisting development, or similar constraints exist.

Finding: The project is not proposing a public walkway and none is required.

This criterion is not applicable.

T. Street Trees. Street trees shall be provided for all projects subject to Type II design review, partition, or subdivision. Street trees shall be installed in accordance with the provisions of NMC 15.420.010(B)(4).

Finding: Lot 07000 where the modular is being placed does not border any public streets. The development site, which includes Lots R3218AD 06900, R3218AD 07000, and R3218AD 07100, possess street trees which are currently meeting the street tree requirements pursuant to NMC 15.420.010(B)(4).

This criterion is met.

U. Street Lights. All developments shall include underground electric service, light standards, wiring and lamps for streetlights according to the specifications and standards of the Newberg public works design and construction standards. The developer shall install all such facilities and make the necessary arrangements with the serving electric utility as approved by the city. Upon the city's acceptance of the public improvements associated with the development, the street lighting system, exclusive of utility-owned service lines, shall be and become property of the city unless otherwise designated by the city through agreement with a private utility.

Finding: Because street lights were provided with a previous project, the project satisfies the requirements for provision of street lights for the subject property.

This criterion is met.

V. Transit Improvements. Development proposals for sites that include or are adjacent to existing or planned transit facilities, as shown in the Newberg transportation system plan or adopted local or regional transit plan, shall be required to provide any of the following, as applicable and required by the review authority:

- 1. Reasonably direct pedestrian connections between the transit facility and building entrances of the site. For the purpose of this section, “reasonably direct” means a route that does not deviate unnecessarily from a straight line or a route that does not involve a significant amount of out-of-direction travel for users.***
- 2. A transit passenger landing pad accessible to disabled persons.***
- 3. An easement of dedication for a passenger shelter or bench if such facility is in an adopted plan.***
- 4. Lighting at the transit facility.***

Finding: Because the Applicant is not proposing transit improvements and the site is not adjacent to either existing or planned transit facilities, this criterion is not applicable.

15.505.040 *Public utility standards.*

A. Purpose. The purpose of this section is to provide adequate services and facilities appropriate to the scale and type of development.

B. Applicability. This section applies to all development where installation, extension or improvement of water, wastewater, or private utilities is required to serve the development or use of the subject property.

C. General Standards.

1. The design and construction of all improvements within existing and proposed rights-of-way and easements, all improvements to be maintained by the city, and all improvements for which city approval is required shall conform to the Newberg public works design and construction standards and require a public improvements permit.

2. The location, design, installation and maintenance of all utility lines and facilities shall be carried out with minimum feasible disturbances of soil and site. Installation of all proposed public and private utilities shall be coordinated by the developer and be approved by the city to ensure the orderly extension of such utilities within public right-of-way and easements.

D. Standards for Water Improvements. All development that has a need for water service shall install the facilities pursuant to the requirements of the city and all of the following standards. Installation of such facilities shall be coordinated with the extension or improvement of necessary wastewater and stormwater facilities, as applicable.

1. All developments shall be required to be linked to existing water facilities adequately sized to serve their intended area by the construction of water distribution lines, reservoirs and pumping stations which connect to such water service facilities. All necessary easements required for the construction of these facilities shall be obtained by the developer and granted to the city pursuant to the requirements of the city.

2. Specific location, size and capacity of such facilities will be subject to the approval of the director with reference to the applicable water master plan. All water facilities shall conform with city pressure zones and shall be looped where necessary to provide adequate pressure and fire flows during peak demand at every point within the system in the development to which the water facilities will be connected. Installation costs shall remain entirely the developer's responsibility.

3. The design of the water facilities shall take into account provisions for the future extension beyond the development to serve adjacent properties, which, in the judgment of the city, cannot be feasibly served otherwise.

4. Design, construction and material standards shall be as specified by the director for the construction of such public water facilities in the city.

Finding: The subject property is already connected to municipal services and no new connection to the public water line is proposed within the project.

There is a hydrant at the entrance to the site and the application materials include a fire flow test performed by Wyatt Fire Protect Inc. The application materials also include an approved Service Provider Permit (No. 2023-0049) which was issued following review of the proposed project by Tualatin Valley Fire & Rescue and indicates that no additional inspections or conditions of approval were required by Tualatin Valley Fire & Rescue.

The criteria are met.

E. Standards for Wastewater Improvements. All development that has a need for wastewater services shall install the facilities pursuant to the requirements of the city and all of the following standards. Installation of such facilities shall be coordinated with the extension or improvement of necessary water services and stormwater facilities, as applicable.

1. All septic tank systems and on-site sewage systems are prohibited. Existing septic systems must be abandoned or removed in accordance with Yamhill County standards.

2. All properties shall be provided with gravity service to the city wastewater system, except for lots that have unique topographic or other natural features that make gravity wastewater extension impractical as determined by the director. Where gravity service is impractical, the developer shall provide all necessary pumps/lift stations and other improvements, as determined by the director.

3. All developments shall be required to be linked to existing wastewater collection facilities adequately sized to serve their intended area by the construction of wastewater lines which connect to existing adequately sized wastewater facilities. All necessary easements required for the construction of these facilities shall be obtained by the developer and granted to the city pursuant to the requirements of the city.

4. Specific location, size and capacity of wastewater facilities will be subject to the approval of the director with reference to the applicable wastewater master plan. All wastewater facilities shall be sized to provide adequate capacity during peak flows from the entire area potentially served by such facilities. Installation costs shall remain entirely the developer's responsibility.

5. Temporary wastewater service facilities, including pumping stations, will be permitted only if the director approves the temporary facilities, and the developer provides for all facilities that are necessary for transition to permanent facilities.

6. The design of the wastewater facilities shall take into account provisions for the future extension beyond the development to serve upstream properties, which, in the judgment of the city, cannot be feasibly served otherwise.

7. Design, construction and material standards shall be as specified by the director for the construction of such wastewater facilities in the city.

Finding: No new connection to the public wastewater line is proposed. Because the site is already connected to the public wastewater line, the standards for wastewater connections and improvements are fulfilled.

This criterion is met.

F. Easements. Easements for public and private utilities shall be provided as deemed necessary by the city, special districts, and utility companies. Easements for special purpose uses shall be of a width deemed appropriate by the responsible agency. Such easements shall be recorded on easement forms approved by the city and designated on the final plat of all subdivisions and partitions. Minimum required easement width and locations are as provided in the Newberg public works design and construction standards. [Ord. 2810 § 2 (Exhs. B, C), 12-19-16.]

Finding: N College Street is under the jurisdiction of the Oregon Department of Transportation (ODOT). There is a public utility easement benefiting ODOT along the property frontage. Because the site possesses easements adequate for utility services and roadway access, no further easements are required.

This criterion is met.

15.505.050 *Stormwater system standards.*

A. Purpose. *The purpose of this section is to provide for the drainage of surface water from all development; to minimize erosion; and to reduce degradation of water quality due to sediments and pollutants in stormwater runoff.*

B. Applicability. *The provisions of this section apply to all developments subject to site development review or land division review and to the reconstruction or expansion of such developments that increases the flow or changes the point of discharge to the city stormwater system. Additionally, the provisions of this section shall apply to all drainage facilities that impact any public storm drain system, public right-of-way or public easement, including but not limited to off-street parking and loading areas.*

C. General Requirement. *All stormwater runoff shall be conveyed to a public storm wastewater or natural drainage channel having adequate capacity to carry the flow without overflowing or otherwise causing damage to public and/or private property. The developer shall pay all costs associated with designing and constructing the facilities necessary to meet this requirement.*

Finding: The proposed development will create more than 500 square feet of impervious area. The Applicant has submitted a LIDA sizing form to determine the size of facility needed to treat and detain the 1,792 square feet of new impervious area. The Applicant plans to increase the size of an existing facility to accommodate the new run off. The existing facility drains to the 12-inch private storm line which drains to a catch basin that connects to a public storm line in Pinehurst Drive. The Applicant shall submit final plans for building permits with a low-impact development approach (LIDA) sizing form.

This criterion will be met if the aforementioned condition of approval is adhered to.

D. Plan for Stormwater and Erosion Control. *No construction of any facilities in a development included in subsection (B) of this section shall be permitted until an engineer registered in the State of Oregon prepares a stormwater report and erosion control plan for the project. This plan shall contain at a minimum:*

- 1. The methods to be used to minimize the amount of runoff, sedimentation, and pollution created from the development both during and after construction.*
- 2. Plans for the construction of stormwater facilities and any other facilities that depict line sizes, profiles, construction specifications, and other such information as is necessary for the city to review the adequacy of the stormwater plans.*
- 3. Design calculations shall be submitted for all drainage facilities. These drainage calculations shall be included in the stormwater report and shall be stamped by a licensed professional engineer in the State of Oregon. Peak design discharges shall be computed based upon the design criteria outlined in the public works design and construction standards for the city.*

Finding: The stormwater management plan and erosion control plan submitted was prepared by a professional engineer licensed in the State of Oregon. The Applicant will be required to obtain a City of Newberg Erosion Control Permit prior to any ground disturbing activity.

This criterion will be met if the aforementioned condition of approval is adhered to.

E. Development Standards. Development subject to this section shall be planned, designed, constructed, and maintained in compliance with the Newberg public works design and construction standards. [Ord. 2810 § 2 (Exhs. B, C), 12-19-16.]

Finding: Because the Applicant has not provided construction plans, the Applicant will be required to provide detailed construction plans that address requirements outlined in the Public Works Design and Construction Standards in accordance with NMC 13.25 Stormwater Management.

A private maintenance agreement for the stormwater facility will be required. Because a private maintenance agreement for the stormwater facility has not been recorded, the Applicant will submit a private maintenance agreement for the on-site stormwater facility and have the approved agreement recorded.

This criterion will be met if the aforementioned condition of approval is adhered to.

Conclusion: The proposed site design review meets the applicable review criteria for a Type II Design Review.

Section III: Conditions of Approval – DR223-0002

C.S. Lewis Academy – New Modular Building

Site Design Review – DR223-0002/ADJC23-0004 – 1605 N College Street

THE FOLLOWING MUST BE COMPLETED BEFORE THE CITY WILL ISSUE A BUILDING PERMIT:

1. **ADA Accessible Connection to Pedestrian Pathway:** The Applicant shall be required to provide a connection that meets the applicable building code and Americans with Disabilities Act requirements for access from the existing pedestrian pathway to the proposed modular classroom during the building permit process and prior to occupancy.
2. **Fire Flow Test Results:** The fire flow test results performed by Wyatt Fire Protect Inc. shall be submitted with the building permit application to be reviewed by the Fire Marshall for approval.
3. **Public Improvement Permit Required:** Any required public improvement permit(s) for this project must be submitted, approved, and issued prior to building permits being issued.
4. **LIDA Sizing Form:** The Applicant shall submit final plans for building permits with a low-impact development approach (LIDA) sizing form.
5. **Erosion Control Permit Required:** The Applicant will be required to obtain a City of Newberg Erosion Control Permit prior to any ground disturbing activity.
6. **Public Works Design and Construction Standards – Stormwater Management:** The Applicant will be required to provide detailed construction plans that address requirements outlined in the Public Works Design and Construction Standards in accordance with NMC 13.25 Stormwater Management.
7. **Private Maintenance Agreement – On-site stormwater Facility:** The Applicant will submit a private maintenance agreement for the on-site stormwater facility and have the approved agreement recorded.

Attachment 1: Application

TYPE II APPLICATION – LAND USE

File #: DR223-0002

TYPES – PLEASE CHECK ONE:

- Design review
- Tentative Plan for Partition
- Tentative Plan for Subdivision
- Type II Major Modification
- Variance _____
- Other: (Explain) _____

APPLICANT INFORMATION:

APPLICANT: Yamhill LAnd Developemnt Services LLC

ADDRESS: PO Box 1042 CITY: Newberg STATE: OR ZIP: 97132

EMAIL ADDRESS: djd.ylds@gmail.com PHONE: _____ MOBILE: 503-476-7702

OWNER (if different from above): Pacific Region Open Bible Standard Churches PHONE: _____

ADDRESS: 1605 N College St CITY: Newberg STATE: OR ZIP: 97132

ENGINEER/SURVEYOR: Yamhill Land Development Services LLC CONTACT: Daniel Danicic, PE50.00050.000

EMAIL ADDRESS: djd.ylds@gmail.com PHONE: _____ MOBILE: 503-476-7702

GENERAL INFORMATION:

PROJECT LOCATION: 1605 N College St Newberg, OR 97132 PROJECT VALUATION: \$ 50,000

PROJECT DESCRIPTION/USE: Placement of Modular Classroom

MAP/TAX LOT NO. (i.e. 3200AB-400): R3216AD-06900, 07000 and 07400 SITE SIZE: 3.54 SQ. FT. ACRE

COMP PLAN DESIGNATION: LDR CURRENT ZONING: R-1

CURRENT USE: School

SURROUNDING USES:

NORTH: R-2 Residential SOUTH: R-1 Residential

EAST: R-1 Residential WEST: R-1 Residential

ATTACHED PROJECT CRITERIA AND REQUIREMENTS (check all that apply)

General Checklist: Fees Public Notice Information Current Title Report Written Criteria Response Owner Signature

2 Copies of full Application Packet

For detailed checklists, applicable criteria for the written response, and other requirements per application type, turn to:

Design Review	p. 13
Partition Tentative Plat	p. 15
Subdivision Tentative Plat	p. 17
Variance Checklist	p. 20
Short-term Rental	p. 22

The Application Packet can be submitted to Planning@newbergoregon.gov or at 414 E First St., Newberg OR. 97132
 If the Application is emailed 2 physical copies must be mailed or brought into the Community Development Department

The above statements and information herein contained are in all respects true, complete, and correct to the best of my knowledge and belief. Tentative plans must substantially conform to all standards, regulations, and procedures officially adopted by the City of Newberg. All owners must sign the application or submit letters of consent. Incomplete or missing information may delay the approval process.

[Signature] 3-23-23
 Applicant Signature Date

[Signature] 3-20-23
 Owner Signature Date

Daniel Danicic, PE
 Print Name

Steve Wallo
 Print Name

Type I Application (Administrative Review)

File #: DR223-0002 / ADJC 23-6004

TYPES – PLEASE CHECK ONE:

- | | |
|---|--|
| <input checked="" type="checkbox"/> Code Adjustment | <input type="checkbox"/> Property Line Consolidation |
| <input type="checkbox"/> Final Plat | <input type="checkbox"/> Type I Extension or Type I Minor/Major Modification |
| <input type="checkbox"/> Minor Design Review | <input type="checkbox"/> Type II or Type III Extension or Minor Modification |
| <input type="checkbox"/> Property Line Adjustment | |
| <input type="checkbox"/> ADU or Cottage Cluster Design Review | <input type="checkbox"/> Other: (Explain) _____ |

APPLICANT INFORMATION:

APPLICANT: Yamill Land Development Services LLC

ADDRESS: PO Box 1042 CITY: Newberg STATE: OR ZIP: 97132

EMAIL ADDRESS: djd.ylds@gmail.com PHONE: _____ MOBILE: 503-476-7702

OWNER (if different from above): Pacific Region Open Bible Standard Churches PHONE: _____

ADDRESS: 1605 N College St CITY: Newberg STATE: OR ZIP: 97132

ENGINEER/SURVEYOR: Yamill Land Development Service LLC CONTACT: Daniel Danicic, PE

EMAIL ADDRESS: djd.ylds@gmail.com PHONE: _____ MOBILE: 503-476-7702

GENERAL INFORMATION:

PROJECT LOCATION: 1605 N College St Newberg OR 97132 PROJECT VALUATION: \$ 50,000

PROJECT DESCRIPTION/USE: Placement of Modular Classroom

MAP/TAX LOT NO. (i.e. 3200AB-400): R3216AD-06900, 7100, & 7400 SITE SIZE: 3.54 SQ. FT. ACRE

COMP PLAN DESIGNATION: LDR CURRENT ZONING: R-1

CURRENT USE: School

SURROUNDING USES:

NORTH: R-2 Residential SOUTH: R-1 Residential

EAST: R-1 Residential WEST: R-1 Residential

ATTACHED PROJECT CRITERIA AND REQUIREMENTS (check all that apply)

General Checklist: Fees Current Title Report Written Criteria Response Owner Signature 2 Copies of full Application Packet

For detailed checklists, applicable criteria for the written criteria response, and number of copies per application type, turn to:

Code Adjustment.....	p. 4
Final Plat	p. 6
Minor Design Review	p. 10
Property Line Consolidation.....	p. 11
Property Line Adjustment.....	p. 12

The above statements and information herein contained are in all respects true, complete, and correct to the best of my knowledge and belief. Tentative plans must substantially conform to all standards, regulations, and procedures officially adopted by the City of Newberg. All owners must sign the application or submit letters of consent. Incomplete or missing information may delay the approval process.

[Signature] 3/20/23
Applicant Signature Date

[Signature] 3/20/23
Owner Signature Date

Daniel Danicic, PE
Print Name

Steve Wallo
Print Name

Date: March 15, 2023
To: City of Newberg, Planning Dept.
From: Daniel Danicic, PE
Subject: CS Lewis Academy Modular Classroom Addition

Applicant: CS Lewis Academy
1605 N. College St
Newberg, OR 97132

Prepared by: Daniel Danicic, PE
Yamhill Land Development Services LLC

Location: 1605 N. College St
Newberg, OR 97132

Tax Map ID: R3218AD 06900, 07000 and 07100

Zoning: R1 Low Density Residential
Tax Lot R3218AD-07100 and 06900

Project Overview:

CS Lewis Academy is proposing to place a pre-manufactured modular classroom unit on their property. It will be placed next to the modular unit placed this last year.

The unit is an existing structure being donated by the Newberg School District. It is currently located on the Edwards School site. The modular unit is 1792 SF and consists of two classrooms each with a single toilet and sink. It will be plumbed into the onsite private water and sanitary sewer lines. The original set of building plans will be submitted for approval when the building permit is submitted.

The rain garden constructed for the recent modular unit installation will be expanded to serve the new unit.

Date: March 15, 2023
To: City of Newberg, Planning Dept.
From: Daniel Danicic, PE
Subject: CS Lewis Academy Modular Classroom Addition

1. Design Compatibility. The proposed design review request incorporates an architectural design which is compatible with and/or superior to existing or proposed uses and structures in the surrounding area. This shall include, but not be limited to, building architecture, materials, colors, roof design, landscape design, and signage.

RESPONSE: The proposed architectural design of the modular building is wood siding with asphalt shingle roofing. This is very similar to the residential homes to the north, the adjacent modular unit and the other classroom and administrative offices on the site. The adjacent auditorium is metal sided. The structure will fit in well with the overall site improvements.

2. Parking and On-Site Circulation. Parking areas shall meet the requirements of NMC 15.440.010. Parking studies may be required to determine if adequate parking and circulation are provided for uses not specifically identified in NMC 15.440.010. Provisions shall be made to provide efficient and adequate on-site circulation without using the public streets as part of the parking lot circulation pattern. Parking areas shall be designed so that vehicles can efficiently enter and exit the public streets with a minimum impact on the functioning of the public street.

RESPONSE:

Circulation: On-site circulation consists of two entrances from N College St; one in and one out. This layout does not utilize the public street and provides an efficient entry and exit onto the public street. The circulation is shown in the figure below:



Parking: A Type I Code Adjustment application is being submitted simultaneously with this application in regard to the number of parking spaces on the site. Parking spaces are provided by the existing site improvements. NMC 15.440.030 requires 104 spaces with the new modular classroom unit. Current parking spaces number 91 total, 88 standard and 3 ADA spaces, resulting in a 14% decrease in the requisite number of spaces which is allowed by NMC..

3. Setbacks and General Requirements. The proposal shall comply with NMC 15.415.010 through 15.415.060 dealing with height restrictions and public access; and NMC 15.405.010 through 15.405.040 and 15.410.010 through 15.410.070 dealing with setbacks, coverage, vision clearance, and yard requirements

RESPONSE:

15.410.020 Font yard setback does not apply as this location of the improvement is set in the interior of the lot with no right of way frontage.

15.10.030 Interior setback minimum of five feet is met as the proposed structure is set 41.40 feet from the north property line.

15.415.020 Building height limitation of 30 feet is met as the proposed structure is 14.5 feet.

4. Landscaping Requirements. The proposal shall comply with NMC 15.420.010 dealing with landscape requirements and landscape screening.

RESPONSE:

15.420.010 A minimum of 15% landscaped area is required.

Existing property area 154,085 SF; Required landscaped area of 15% is 23,112 SF

Existing Building area is 35,042 SF plus additional 1,770 SF for proposed modular unit results in a total building coverage of 24%

The existing parking area is 40,478 SF for a total coverage of 26%

The remain lot area not improved is landscaped/natural ground totals 76,525 sf which is 50%. Landscape criteria is met.

5. Signs. Signs shall comply with NMC 15.435.010 et seq. dealing with signs.

RESPONSE: Not applicable.

6. Manufactured Dwelling, Mobile Home and RV Parks. Manufactured dwelling and mobile home parks shall also comply with the standards listed in NMC 15.445.075 through 15.445.100 in addition to the other clear and objective criteria listed in this section. RV parks also shall comply with NMC 15.445.170 in addition to the other criteria listed in this section.

RESPONSE: Not applicable.

7. Zoning District Compliance. The proposed use shall be listed as a permitted or conditionally permitted use in the zoning district in which it is located as found in NMC 15.305.010 through 15.336.020. Through this site review process, the director may make a determination that a use is determined to be similar to those listed in the applicable zoning district, if it is not already specifically listed. In this case, the director shall make a finding that the use shall not have any different or more detrimental effects upon the adjoining neighborhood area than those specifically listed.

RESPONSE: The existing zoning of R1 allows for educational classrooms.

8. Sub district Compliance. Properties located within subdistricts shall comply with the provisions of those subdistricts located in NMC 15.340.010 through 15.348.060.

RESPONSE: Not applicable.

9. Alternative Circulation, Roadway Frontage Improvements and Utility Improvements. Where applicable, new developments shall provide for access for vehicles and pedestrians to adjacent properties which are currently developed or will be developed in the future. This may be accomplished through the provision of local public streets or private access and utility easements. At the time of development of a parcel, provisions shall be made to develop the adjacent street frontage in accordance with city street standards and the standards contained in the transportation plan. At the discretion of the city, these improvements may be deferred through use of a deferred improvement agreement or other form of security.

RESPONSE: The entire street frontage was recently improved as part of a City of Newberg pedestrian improvements project constructing new sidewalk, curb and driveway approaches. The improvements meet all ODOT and City standards.

10. Traffic Study Improvements. If a traffic study is required, improvements identified in the traffic study shall be implemented as required by the director.

RESPONSE: From the Institute of Transportation Engineers (ITE), Trip Generation Manual, 10th Edition, Trip Code 520 Elementary School, the PM peak hour vehicle trip rate is 3.16 per 1,000 square feet of gross floor area. For the proposed modular addition of 1,792 square feet this equates to 5.7 pm peak hour vehicle trips. This is less than the 40 trip requirement to trigger a Traffic Study.

EXTERIOR LIGHTING Chapter 15.425.

RESPONSE: There are no proposed changes or additions to the existing area lighting on the site. The proposed classroom unit will only have entry lighting at each doorway.

STORMWATER

The overall site drainage is to the west. The improved portion of the site, buildings and parking lot, drain southeasterly to a low point catch basin in the parking lot along the south property line. This catch basin drains via a 12" concrete pipe south to a catch basin in Pinehurst Dr.

The project creates a net impervious area of 1770 SF which exceeds the 500 square feet trigger for stormwater treatment per 13.25.260. The proposed modular building is 1,770 square feet. If the new impervious area is greater than 500 square feet and less than 2,877 square feet, Best Management Practices Application and Sizing Forms may be used.

A stormwater garden sized using the city LIDA sizing form will be used to provide stormwater treatment for the new structure. A rain garden was recently constructed for a similar sized modular classroom unit. This facility will be increased in size to accommodate the new modular unit.

WATER AND WASTEWATER

The proposed modular unit has two bathroom facilities. These will be plumbed into the existing onsite private water and sanitary sewer system. No new public service lines or laterals are needed.

FIRE SERVICE AND FLOW

There is a hydrant at the street entrance to the site, west side of N. College St. The results of a flow test by Wyatt Fire Protect, Inc on 7/12/21 are:

FLOW	STATIC	86psi
	PITOT RESIDUAL	27psi
	GPM	1954
	GPM AT 20 PSI	7132 7% drop calculated per NFPA 291
MONITOR	STATIC	86psi
	RESIDUAL	80psi

TRANSPORTATION

N. College is classified as a minor arterial in the Newberg TSP. The required minimum right of way is 69ft. which is a 34.5ft. half street width. The existing half street right of way is 35 ft meeting the standard. The required frontage improvements are for 6ft bike lane, curb, 5.5ft planter strip and 5ft sidewalk. This standard is met except for the planter strip as a portion of the frontage is curb-tight sidewalk. The City of Newberg recently completed these improvements and can be considered in full compliance.

Date: March 15, 2023
To: City of Newberg, Planning Dept.
From: Daniel Danicic, PE
Subject: CS Lewis Academy ModulaR Classroom Addition

Pursuant to NMC 15.210.020, Applicant is requesting a code adjustment to the requisite number of parking spaces in conjunction with its type II application for a major site design review.

Chapter 15.210 Code Adjustments
15.210.020 Type I adjustments and approval criteria.

(C) dimensional standards and minimum number of off-street parking spaces, Provide a written response to how your project will meet the following criteria:

(a) Special characteristics of users which indicate low demand for off-street parking (e.g. low income, elderly).

RESPONSE: The majority of vehicle trips to the school is to drop off and pick up students. The parking area is primarily used by staff and visitors. It is observed that the lot routinely has excess capacity.

(b) Opportunities for joint use of nearby off-street parking facilities.

RESPONSE: There are no nearby locations that could provide the opportunity to use off street parking facilities jointly.

(c) Availability of public transit.

RESPONSE: Public transit runs along N College Street and is readily available to serve the subject property.

(d) Natural features of the site (topography, vegetation, drainage) which would be adversely affected by application of required parking standards.

RESPONSE: No such natural features exist.

(e) Possible conversion of the site to other uses in the future.

RESPONSE: Future development of the property to the west would allow for additional parking and structures. It would be more appropriate to require the additional parking with future more significant development of the site.

(f) No adjustment shall be greater than 25% of the requirement from which the exception is granted.

RESPONSE: Parking spaces are provided by the existing site improvements. NMC 15.440.030 requires 115 spaces with the new modular classroom unit. Current parking spaces number 88 total, 85 standard and 3 ADA spaces, resulting in an 25% decrease in the requisite number of spaces which is allowed by NMC.

Category	Number of Teaching Stations	Parkings Spaces per Stations	Total Spaces
High School (existing)	4	9.5	38
Middle School (existing)	3	5.5	16.5
Elementary School (existing)	7	5.5	38.5
Middle School (New proposed)	2	5.5	11
	14	Total Spaces Required	104
		14% Reduction	14
		Min Number of Spaces with 14% reduction	90
		Total Existing Spaces	91

With the 14% code adjustment, there will be a sufficient number of spaces in the existing parking area.

Applicant requests an adjustment to the code to allow for an 14% reduction in the parking requirement for the addition of one modular classroom.

POSTED NOTICE

Land Use Notice

FILE NO. DR223-XXXX/ADJC23-XXX

PROPOSAL: Add one modular classroom building to site

FOR FURTHER INFORMATION, CONTACT:

City of Newberg
Community Development Department
414 E First Street
Phone: 503-537-1240

2'

3'

Notice must be white with black letters, and must be landscape orientation, as shown above.
The notice must be lettered using block printing or a "sans-serif" font, such as Arial.



Community Development Department

P.O. Box 970 • 414 E First Street • Newberg, Oregon 97132

503-537-1240. Fax 503-537-1272 www.newbergoregon.gov

WE WANT YOUR COMMENTS ON A PROPOSED NEW DEVELOPMENT IN YOUR NEIGHBORHOOD

A property owner in your neighborhood submitted an application to the City of Newberg to add a modular classroom to its current school facilities. You are invited to take part in the City's review of this project by sending in your written comments. The applicable criteria used to make a decision on this application for design review and code adjustment approval are found in the Newberg Development Code 15.15.220 and 15.210.

For more details about giving comments, please see the back of this sheet.

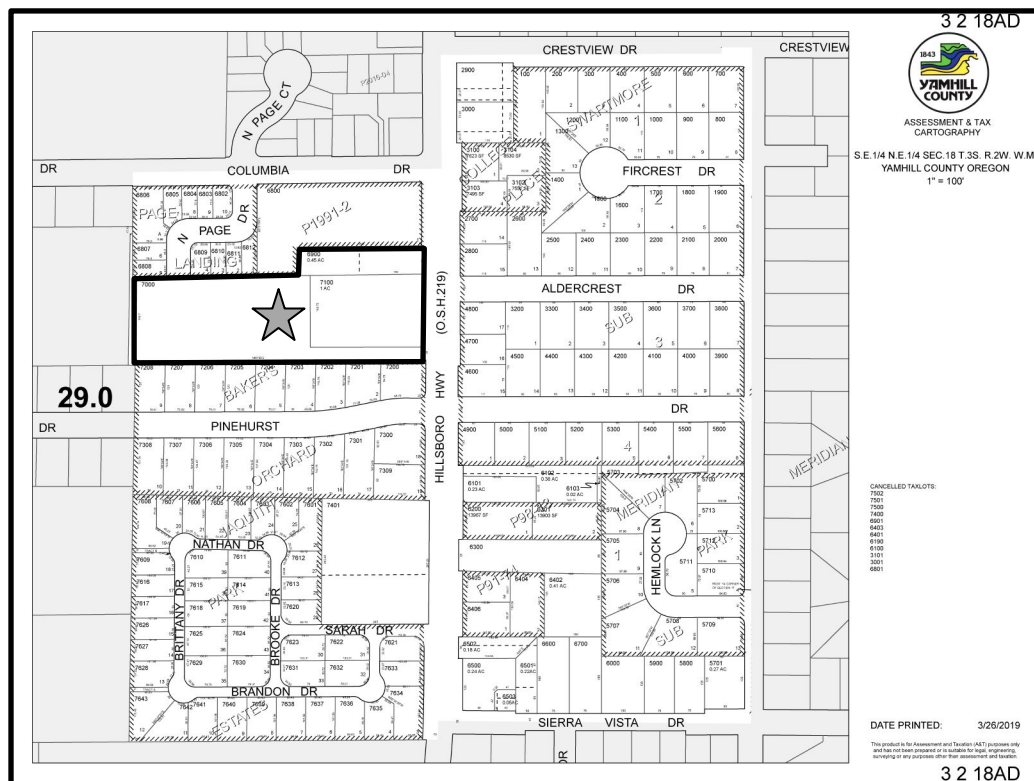
The application would add one more modular classroom to the existing school facilities.

APPLICANT: *Daniel Danicic, Yamhill Land Development LLC*
TELEPHONE: *503-476-7702*

PROPERTY OWNER: *Pacific Region Open Bible Standard Churches*

LOCATION: *1605/1701 N. College Newberg, OR 97132*

TAX LOT NUMBER: *Yamhill County Tax Map and Lot Number 3218AD-06900, 07000 and 07100*



Working Together For A Better Community-Serious About Service"

K:\WP\PLANNING\BOBBIE\Working on\Notice templates working on\DR Sample Notice.doc

We are mailing you information about this project because you own land within 500 feet of the proposed new project. We invite you to send any written comments for or against the proposal within 14 days from the date this notice is mailed.

If you mail your comments to the City, please put the following information on the outside of the envelope:

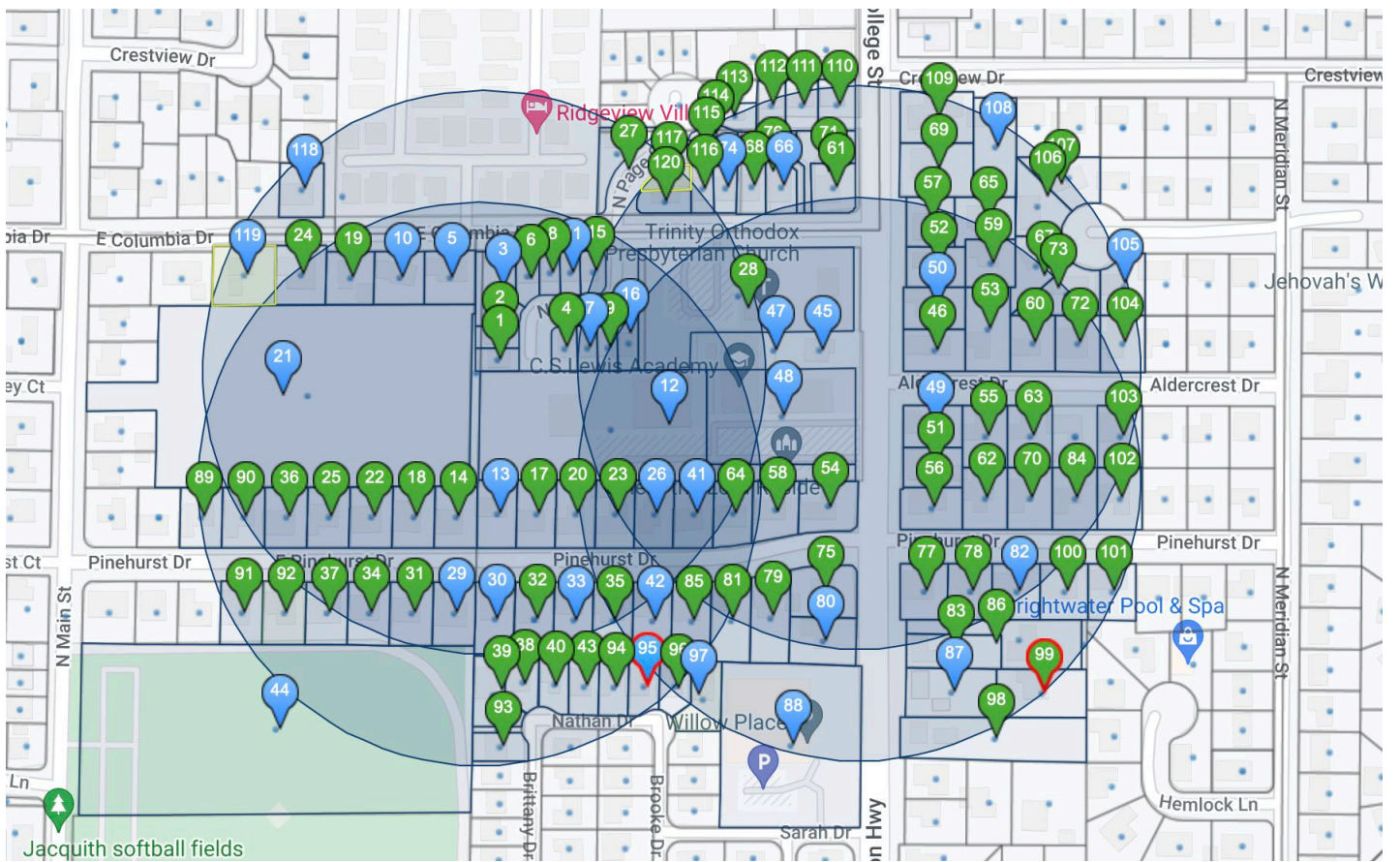
Written Comments: File No.XX **(City staff will give you the file number for
City of Newberg your project at the time of application)**
Community Development Department
PO Box 970
Newberg, OR 97132

You can look over all the information about this project or drop comments off at Newberg City Hall, 414 E. First Street. You can also buy copies of the information for a cost of 25 cents a page. If you have any questions about the project, you can call the Newberg Planning Division at 503-537-1240.

All written comments must be turned in by 4:30 p.m. on **enter date two weeks from date you mailed notice**. Any issue which might be raised in an appeal of this case to the Land Use Board of Appeals (LUBA) must be submitted to the City in writing before this date. You must include enough detail to enable the decision maker an opportunity to respond. The applicable criteria used to make a decision on this application for design review approval are found in Newberg Development Code 15.220.050(B).

The Community Development Director will make a decision at the end of a 14-day comment period. If you send in written comments about this project, you will be sent information about any decision made by the City relating to this project.

Date Mailed: ***Date notice is mailed***





First American

my**FirstAm**[®]

1 Line Report

Search Criteria:

Farm Type: Parcel
Land Use Type: Both
Property Type: Residential Properties: All Residential Types
Residential Income & Other: All Res Income And Other Types
Commercial, Industrial, Government/Public Use, Agricultural/Rural, Recreational/Entertainment,
Transportation/Communication
Vacant Land: All Vacant Types
Res. Beds: No Min to No Max
Res. Baths: No Min to No Max
Comm. Stories:
Comm. # of Units: No Min to No Max
Sale Price: No Min to No Max
Sq. Ft.: No Min to No Max
Lot Size: No Min to No Max
Pool: No Preference
Months Back: Any
Occupancy: All



First American

myFirstAm®

1 Line Report

	Parcel Number	Owner Name	Site Address	BD/BA	SqFt	YB
1	39862	Acosta Guillermo Acosta Vicki	200 E Columbia Dr Newberg 97132	5/3	2967	1985
2	41653	Adams Ethan Shae Hill	1400 N College St Newberg 97132	2/1	868	1947
3	561742	Alfaro Francisco J Cazares	1827 N College St Newberg 97132	3/2	1746	2016
4	39942	Anderson Lonny L Anderson	109 E Pinehurst Dr Newberg 97132	3/2	1624	1979
5	41074	Ankeny Nathanael S Ankeny	705 E Aldercrest Dr Newberg 97132	3/1	960	1969
6	40958	Baltzell Todd E Baltzell Cheryl	708 E Fircrest Dr Newberg 97132	3/1	1024	1983
7	490240	Barajas Gregorio C Barajas		3/2	1331	1993
8	711210	Barnett Rhonda L	1812 N Page Ct Newberg 97132	3/3	1668	2019
9	41065	Bauer George R Bauer	709 E Aldercrest Dr Newberg 97132	3/1	960	1970
10	41083	Bellingar Enterprises Llc	1704 N College St Newberg 97132	3/1	1152	1971
11	561739	Bender Tyler Carter Alana C	1821 N College St Newberg 97132	3/1	1784	2016
12	485701	Benuche Joseph E Benuche	409 E Nathan Dr Newberg 97132	3/2	1054	1993
13	42019	Berryman Teddy M Berryman	400 E Pinehurst Dr Newberg 97132	3/1	1074	1977
14	41984	Bessler Brian D Trustee	500 E Pinehurst Dr Newberg 97132	3/2	1697	1977
15	41305	Buckhalter Robert J	709 E Pinehurst Dr Newberg 97132	2/2	1493	1969
16	39835	Byrum Rosa L	300 E Columbia Dr Newberg 97132	3/2	1500	1982
17	41902	Carey Deanna	409 E Pinehurst Dr Newberg 97132	3/2	1232	1977
18	711220	Carlstrom Evan S Carlstrom	1802 N Page Ct Newberg 97132	3/3	1492	2019
19	40976	Carsley Betty J Darby	800 E Fircrest Dr Newberg 97132	3/1	1344	1972
20	485704	Carstensen Wayne T	407 E Nathan Dr Newberg 97132	3/2	1157	1993
21	527398	Cazarez Carlos V Velazquez	1794 N College St Newberg 97132	4/2	1738	2004
22	40397	Chehalem Park & Recreation	1414 N Main St Newberg 97132	0/0	0	0
23	41421	Coatney Carol A	800 E Pinehurst Dr Newberg 97132	2/1	960	1969
24	398965	Colnu Llc	308 E Columbia Dr Newberg 97132	3/2	1693	1980
25	41341	Cornick Living Trust Cornick	700 E Aldercrest Dr Newberg 97132	3/1	1252	1971
26	41323	Correa Ramon	701 E Pinehurst Dr Newberg 97132	3/2	1490	1968
27	41975	Currier Roger E Currier Josie	504 E Pinehurst Dr Newberg 97132	3/1	1074	1977
28	485719	Davis Derris Davis Emily	408 E Nathan Dr Newberg 97132	4/2	1652	1992
29	39853	Decker Susan	220 E Columbia Dr Newberg 97132	2/1	1106	1940
30	40770	Dendy Scott J Dendy Christina	704 E Crestview Dr Newberg 97132	3/1	1198	1968
31	40949	Devoe Sherry A	709 E Fircrest Dr Newberg 97132	3/1	1344	1972
32	42000	Dewar Tara	404 E Pinehurst Dr Newberg 97132	3/2	1123	1977
33	41378	Doran Bradley J Doran	700 E Pinehurst Dr Newberg 97132	3/2	1618	1968
34	485698	Drill Mary E	411 E Nathan Dr Newberg 97132	3/2	1292	1993
35	41118	Duran Rafael Duran Stacy J	1804 N College St Newberg 97132	3/2	1637	1954
36	40039	Edgy Jason Edgy Elizabeth	305 E Pinehurst Dr Newberg 97132	3/2	1598	1987
37	41332	Eichstadt Jarred	1604 N College St Newberg 97132	3/1	1262	1969
38	39979	Englen Michael G Englen	209 E Pinehurst Dr Newberg 97132	3/2	3561	1980



	Parcel Number	Owner Name	Site Address	BD/BA	SqFt	YB
39	41840	Erickson John A Erickson	1603 N College St Newberg 97132	3/2	2067	1976
40	399214	Farr Peter J Farr Danielle	1506 N College St Newberg 97132	4/2	1648	1981
41	711218	Fernandez Living Trust	15009 Ne North Valley Rd Newberg 97132	1/3	1683	2019
42	41092	Gorsek Anthony J Gorsek	701 E Aldercrest Dr Newberg 97132	3/2	1431	1970
43	532543	Grey Valerie A	607 E Columbia Dr Newberg 97132	3/3	1889	2006
44	41886	Gueldner Ella Trustee For	505 E Pinehurst Dr Newberg 97132	3/2	1123	1977
45	41877	Gueldner Family Llc	509 E Pinehurst Dr Newberg 97132	3/1	1072	1977
46	41109	Hall John Manhat Beth A	1812 N College St Newberg 97132	3/1	1192	1954
47	41136	Harris Mark R Harris Penny J	704 E Aldercrest Dr Newberg 97132	3/2	1238	1970
48	511267	Hartman Kenneth W Haragan	1508 N College St Newberg 97132	4/3	2518	1998
49	711232	Hartman Mason G Hartman	1707 N Page Dr Newberg 97132	4/3	1804	2019
50	711216	Her Ger Xiong Ma Tha	505 E Columbia Dr Newberg 97132	4/3	1824	2019
51	41387	Hess Nickolaus J Reed Olivia	704 E Pinehurst Dr Newberg 97132	3/2	1296	1970
52	41396	Hills Michael Hills Dawn	708 E Pinehurst Dr Newberg 97132	3/1	960	1970
53	39880	Howard Roscoe Howard	110 E Columbia Dr Newberg 97132	2/1	710	1925
54	527401	Johnson Randy S Johnson	1790 N College St Newberg 97132	3/3	1678	2004
55	485725	Kasuba Tina K Trust Kasuba		3/2	1084	1992
56	711222	Katsufrakis Scott A	1727 N Page Dr Newberg 97132	3/3	1668	2019
57	40878	Kimmel Nathan W	605 E Columbia Dr Newberg 97132	3/2	1775	2006
58	711208	Kitchen Jeremy S Kitchen	1203 S Meridian St Newberg 97132	3/3	1509	2019
59	40011	Koch Samuel L Koch	301 E Pinehurst Dr Newberg 97132	3/2	1809	1980
60	40164	Kuehne Harold R Trustee For	308 E Pinehurst Dr Newberg 97132	3/2	1634	1981
61	41261	Lackner Ryan Merckling	801 E Pinehurst Dr Newberg 97132	4/1	1032	1969
62	527395	Laine Andrew Laine Amanda	1800 N College St Newberg 97132	3/2	1679	2004
63	41948	Lamberty Janis E Lamberty	604 E Pinehurst Dr Newberg 97132	3/2	1123	1977
64	40967	Lamm Gregg T Lamm Teresa	712 E Fircrest Dr Newberg 97132	3/2	1344	1972
65	532549	Laperriere Monique M	1801 N College St Newberg 97132	4/1	1796	1949
66	41939	Lapinski Sonja K Moeller Alex	1601 N College St Newberg 97132	2/1	1662	1920
67	711192	Lillebo Gabriel Lillebo Sarah	1809 N Page Ct Newberg 97132	3/3	1620	2019
68	41635	Louthan Gabriel Louthan	1502 N College St Newberg 97132	3/2	1747	1948
69	711226	Lynch Janessa J Lynch James	1719 N Page Dr Newberg 97132	3/3	1668	2019
70	485707	Magoon Shawn E Magoon	405 E Nathan Dr Newberg 97132	3/2	1108	1992
71	532540	Mcadoo Bryan W Mcadoo	603 E Columbia Dr Newberg 97132	3/3	1878	2006
72	39951	Mcdaniel Living Trust	201 E Pinehurst Dr Newberg 97132	3/2	1660	1979
73	41920	Mckone Randy & Debbie	401 E Pinehurst Dr Newberg 97132	3/1	1072	1977
74	532546	Mead Cynthia L	609 E Columbia Dr Newberg 97132	3/3	1769	2006
75	485710	Mini Martha P Ramirez	401 E Nathan Dr Newberg 97132	3/2	1228	1993
76	711234	Moore Thomas P Hansen-	1703 N Page Dr Newberg 97132	3/3	1620	2019



	Parcel Number	Owner Name	Site Address	BD/BA	SqFt	YB
77	40100	Morelli Aaron M Morelli	200 E Pinehurst Dr Newberg 97132	3/2	1687	1980
78	41993	Myers Aaron C Myers Beverly	408 E Pinehurst Dr Newberg 97132	3/1	1072	1977
79	41859	Navarrete Anthony Navarrete	605 E Pinehurst Dr Newberg 97132	3/1	1056	1977
80	41966	Nelson Peter S	508 E Pinehurst Dr Newberg 97132	3/1	1072	1977
81	482491	Newberg Alf Llc	1307 N College St Newberg 97132	0/0	0	0
82	711230	Newberg City Of		0/0	0	0
83	711194	Nguyen Thao M		3/3	1509	2019
84	40146	Norman Dolores M Trustee	300 E Pinehurst Dr Newberg 97132	3/2	1378	1981
85	41289	Oconnor Adeline Trustee Of	713 E Pinehurst Dr Newberg 97132	3/1	960	1970
86	41056	Ouellette Eric Ouellette Susan	713 E Aldercrest Dr Newberg 97132	4/2	1905	1970
87	41813	Pacific Region Open Bible	1701 N College St Newberg 97132	3/2	1906	1912
88	41822	Pacific Region Open Bible		0/0	0	0
89	41831	Pacific Region Open Bible	1605 N College St Newberg 97132	0/0	0	0
90	484198	Pacific Region Open Bible		0/0	0	0
91	714888	Pacific Region Open Bible	1701 N College St Newberg 97132	3/2	1906	1912
92	41957	Parkinson Jack B Parkinson	600 E Pinehurst Dr Newberg 97132	3/1	1072	1977
93	40930	Peters Elise J	713 E Fircrest Dr Newberg 97132	3/1	989	1972
94	711206	Petersen Christian	1818 N Page Ct Newberg 97132	3/3	1620	2019
95	41314	Pierce Ronald E Pierce Donna	705 E Pinehurst Dr Newberg 97132	4/2	1744	1969
96	40119	Piscitelli Pasquale	204 E Pinehurst Dr Newberg 97132	3/2	1542	1986
97	485692	Powell Nathan L Powell Trina	1320 N Brooke Dr Newberg 97132	3/2	1523	1992
98	485716	Quatraro Jeff F Quatraro	1317 N Brittany Dr Newberg 97132	3/2	1657	1993
99	41172	Randall Stuart P Randall	800 E Aldercrest Dr Newberg 97132	4/2	1540	1970
100	41047	Randolph Paul S	801 E Aldercrest Dr Newberg 97132	4/2	1536	1970
101	39960	Reid David C	205 E Pinehurst Dr Newberg 97132	3/2	1495	1979
102	41127	Rogers Sabrina Rogers	1802 N College St Newberg 97132	4/3	2571	1925
103	711240	Rosales Steven R Rosales	1728 N Page Dr Newberg 97132	3/3	1509	2019
104	41868	Rydell Leonard A Schroeder	601 E Pinehurst Dr Newberg 97132	3/2	1792	1977
105	42028	Sanchez Mario	1519 N College St Newberg 97132	3/2	1164	1976
106	399205	Scherer Claudia A	1504 N College St Newberg 97132	2/1	830	1947
107	485695	Senz Family Living Trust Senz	413 E Nathan Dr Newberg 97132	3/2	1122	1993
108	711212	Shine Morgan J	16725 Ne Dopp Rd Newberg 97132	3/3	1492	2019
109	711238	Skopil Homes 4 Llc		3/3	1668	2019
110	711224	Skopil Homes 5 Llc		3/3	1509	2019
111	711242	Skopil Homes 8 Llc		3/3	1668	2018
112	711214	Skopil Homes 9 Llc		3/3	1620	2019
113	41911	Smith Mary H	405 E Pinehurst Dr Newberg 97132	3/1	1072	1977
114	40057	Smits Scott E Smits Rebekah	309 E Pinehurst Dr Newberg 97132	3/2	1300	1981



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1 Line Report

	Parcel Number	Owner Name	Site Address	BD/BA	SqFt	YB
115	41412	Solberg Eric M Solberg Linda	712 E Pinehurst Dr Newberg 97132	3/2	1296	1970
116	41895	Stampley Drew W	501 E Pinehurst Dr Newberg 97132	3/2	1232	1977
117	40155	Stampley Leslie R Revoc	304 E Pinehurst Dr Newberg 97132	3/2	1640	1980
118	711236	Stifter Jason M Stifter Lindsay	1720 N Page Dr Newberg 97132	3/3	1509	2019
119	40814	Tolley Kenneth Tolley Kaylee R	1815 N College St Newberg 97132	3/3	1784	2016
120	715548	Trinity Presbyterian Church Of	600 E Columbia Dr Newberg 97132	0/0	0	0
121	40093	Varney Belinda M	110 E Pinehurst Dr Newberg 97132	3/2	1901	1981
122	485722	Wegener Michael J Wegener	1317 N Brooke Dr Newberg 97132	3/2	1070	1993
123	485713	Whitehurst Taylor	1321 N Brittany Dr Newberg 97132	3/2	1457	1992
124	711228	Willcuts Matthew T	1715 N Page Dr Newberg 97132	3/3	1620	2019
125	41154	Workman-Morelli Michael R	708 E Aldercrest Dr Newberg 97132	4/1	1032	1970
126	711196	Yamada Wayne Yamada		3/3	1668	2019
127	40137	Young Dianne Young David A	208 E Pinehurst Dr Newberg 97132	3/2	1618	1980



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Limitation of Liability for Informational Report

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First American Title Insurance Company

775 NE Evans Street
McMinnville, OR 97128
Phn - (503)376-7363
Fax - (866)800-7294

YAMHILL COUNTY TITLE UNIT
FAX (866)800-7294

Title Officer: Larry Ball
(503)376-7363
lball@firstam.com

LOT BOOK SERVICE

Jessica S Cain Attorney at Law, Cain Law Company
PC
505 Rentfro Way, PO BOX 434
Newberg, OR 97132

Order No.: 1039-3799707

August 17, 2021

Attn: Jessica Cain
Phone No.: (971)998-7507 - Fax No.:
Email: jessica@cainlawcompany.com

Re: CS Lewis Academy

Fee: \$500.00

We have searched our Tract Indices as to the following described property:

PARCEL 1:

Being a portion of Section 18, Township 3 South, Range 2 West of the Willamette Meridian in the City of Newberg Yamhill County, Oregon and being a part of the Donation Land Claim of Oliver J. Walker, Claim No. 53, Notification No. 1706, in Yamhill County, State of Oregon, being more particularly described as follows, to-wit:

Beginning at an iron pipe set in the center of State Highway #219 and on the West line of County Survey #2379, at a point 401.34 feet North and 726 feet West of the Quarter Post between Sections 17 and 18 in said Township and Range; thence North along the center of said highway, 339.40 feet to an iron pipe set at the Southeast corner of a tract conveyed to Peter Hilbert, et us by deed recorded May 27, 1920 in Book 81, Page 90, Yamhill County Deed Records, said corner being the Northeast corner of Lot #2 County Survey #2516-1/2, County Survey Records; thence South 89 degrees 28 minutes West 35 feet to a point on the West margin of said highway, and the true point of beginning of the tract herein described; thence continuing South 89 degrees 28 minutes West along the South line of said Hilbert tract, 267.7 feet; thence South 00 degrees 19 minutes West 162.72 feet to an iron rod; thence North 89 degrees 28 minutes East 267.7 feet to the West margin of said highway; thence North 00 degrees 19 minutes East 162.72 feet to the true place of beginning.

PARCEL 2:

Situate, lying and being in the City of Newberg, Yamhill County, State of Oregon, and being a part of the Donation Land Claim of Oliver J. Walker, Claim No. 53, Notification No. 1706, in Yamhill County, State of Oregon, and part of said claim herein conveyed being particularly described as follows, to-

wit:

Beginning at the Southeast corner of that certain tract of land conveyed by True French and Emma C. French, his wife, to Peter Hilbert and Clara Hilbert, his wife, by deed dated May 25, 1920, and which Deed is now recorded in the Office of the Recorder of Conveyance for Yamhill County, State of Oregon, in Book of Deed Volume 81, Page 90; and running thence West along the South line of said tract so conveyed by True French and wife, to Peter Hilbert and wife, 20 rods; thence North 4 rods; thence East parallel with the South line of the premises herein conveyed, 20 rods; thence South 4 rods to the place of beginning, in Section 18, Township 3 South, Range 2 West of the Willamette Meridian, Yamhill County, Oregon.

and as of 08/13/2021 at 8:00 a.m.

We find that the last deed of record runs to

Pacific Region Open Bible Standard Churches

We find the following apparent encumbrances within ten (10) years prior to the effective date hereof:

NONE

1. City liens, if any, of the City of Newberg.
2. Taxes, including the current fiscal year, have been assessed with Religious exemption. If the exempt status is terminated, additional tax may be levied.

(Affects Parcel 1)
3. The rights of the public in and to that portion of the premises herein described lying within the limits of streets, roads and highways.
4. Easement, including terms and conditions contained therein:
 Granted to: The City of Newberg, a municipal corporation
 For: Water pipe line
 Recorded: February 07, 1941
 Recording Information: Book 117, Page 429, Deed and Mortgage Records
5. Restrictive Covenant to Waive Remonstrance, pertaining to public improvements including the terms and provisions thereof
 Recorded: August 04, 1994 in Film Volume 313, Page 606, Deed and Mortgage Records
6. Deed of Trust and the terms and conditions thereof.
 Grantor/Trustor: Newberg Open Bible Church
 Grantee/Beneficiary: First Federal Savings and Loan - Newberg
 Trustee: David C Haugeberg
 Amount: \$102,700.00
 Dated:
 Recorded: May 15, 2007
 Recording Information: Instrument No. 200710539, Deed and Mortgage Records
 (Affects a portion of Parcel 2)

- 7. Easement, including terms and provisions contained therein:
 Recording Information: March 14, 2014 as Instrument No. 201402847, Deed and Mortgage Records
 In Favor of: State of Oregon, by and through its Department of Transportation
 For: slopes, water, gas, electric and communication service lines, fixtures and facilities

- 8. Unrecorded leases or periodic tenancies, if any.

We have also searched our General Index for Judgments and State and Federal Liens against the Grantee(s) named above and find:

NONE

We find the following unpaid taxes and city liens:

NOTE: Taxes for the year 2020-2021 PAID IN FULL

Tax Amount: \$0.00 EXEMPT
 Map No.: R3218AD 07100
 Property ID: 41831
 Tax Code No.: 29.0

(Affects Parcel 1)

NOTE: Taxes for the year 2020-2021 PAID IN FULL

Tax Amount: \$801.88
 Map No.: R3218AD 06900
 Property ID: 41813
 Tax Code No.: 29.0

(Affects Parcel 2)

THIS IS NOT a title report since no examination has been made of the title to the above described property. Our search for apparent encumbrances was limited to our Tract Indices, and therefore above listings do not include additional matters which might have been disclosed by an examination of the record title. We assume no liability in connection with this Lot Book Service and will not be responsible for errors or omissions therein. The charge for this service will not include supplemental reports, rechecks or other services.



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First American Title Insurance Company
775 NE Evans Street
McMinnville, OR 97128

Illegal Restrictive Covenants

Please be advised that any provision contained in this document, or in a document that is attached, linked, or referenced in this document, that under applicable law illegally discriminates against a class of individuals based upon personal characteristics such as race, color, religion, sex, sexual orientation, gender identity, familial status, disability, national origin, or any other legally protected class, is illegal and unenforceable by law.



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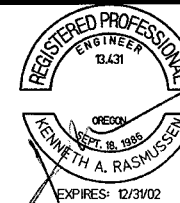
REV.* BY: DATE: PER: SHEETS:

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 JGM 9/13/01 - SUBMIT.

**28' x 64' MODULAR CLASSROOM
 NEWBERG SCHOOL DISTRICT**

NEWBERG, OREGON

COVER SHEET



SHEET **1** OF 19

2001-KM-47-2

GENERAL NOTES

1. THE TERM U.B.C. SHALL APPLY TO THE CURRENT EDITION OF THE O.S.S.C. FOR PROJECTS IN THE STATE OF OREGON AND SHALL APPLY TO THE CURRENT EDITION OF THE AMENDED UNIFORM BUILDING CODE FOR PROJECTS IN THE STATE OF WASHINGTON. FOR PROJECTS IN ALL OTHER STATES IT SHALL APPLY TO THE CURRENT EDITION OF THE UNAMENDED UNIFORM BUILDING CODE.
2. ALL CONTROLS AND HARDWARE SHALL BE ACCESSIBLE TO PERSONS WITH DISABILITIES (109.3)
3. LOCAL JURISDICTION TO DETERMINE COMPLIANCE WITH UBC CHAPTER 29, SECTION 2902 AND TABLE 29A WITH REGARD TO ADJACENT FACILITIES.

P-2

JOB NAME:

NEWBERG S.D.

JOB SITE ADDRESS:

**715 EAST 8th STREET (EDWARDS ELEM.)
 NEWBERG, OR 97132**

OWNER NAME:

NEWBERG SCHOOL DISTRICT

COUNTY:

YAMHILL COUNTY

DESIGN CRITERIA

GENERAL:

CONSTRUCTION TYPE 5-N (NON-SPRINKLERED)
 OCCUPANCY GROUP E1
 OCCUPANCY LOAD (T-10A) 82
 DISTANCE TO PROPERTY LINE OR ASSUMED PROPERTY LINE
 FRONT: 20' MIN.
 REAR: 20' MIN.
 LEFT: 20' MIN.
 RIGHT: 20' MIN.

PLUMBING:

PLUMBING OCCUPANCY LOAD (T-29A) 36
 QUANTITY OF PLUMBING FIXTURES 8

ELECTRICAL:

ELECTRIC SERVICE LOAD 100 AMP + 200 AMP = 300 AMP 1 PH.

MECHANICAL:

VENTILATION OCCUPANCY LOAD 50
 CLIMATE ZONE ZONE 2
 HEATING FAE
 AIR-CONDITIONING YES

STRUCTURAL:

ROOF LIVE LOAD 25 PSF
 FLOOR LIVE LOAD 50 PSF
 WIND LOAD 90 MPH - EXP. B
 SEISMIC ZONE ZONE 3
 OREGON PLAN APPROVAL NO. PENDING

SHEET INDEX

NO.	TITLE
1	COVER SHEET
2	FOUNDATION PLAN
3	FOUNDATION NOTES AND DETAILS
4	FOUNDATION DETAILS
5	FOUNDATION DETAILS
6	FLOOR PLAN
7	SCHEDULES
8	NOTES AND CALCULATIONS
9	EXTERIOR AND INTERIOR ELEVATIONS
10	REFLECTED CEILING / ELECTRICAL / MECHANICAL PLAN
11	ELECTRICAL / MECHANICAL NOTES AND SCHEDULES
12	ELECTRICAL PANEL AND LOAD CALCULATION
13	DETAILS
14	DETAILS
15	DETAILS
16	DETAILS
17	PLUMBING DIAGRAMS
18	CHASSIS PLAN
19	RAMP, LANDING & STAIR DETAILS
20	RAMP, LANDING & STAIR DETAILS
21	RAMP, LANDING & STAIR DETAILS
22	RAMP, LANDING & STAIR DETAILS
23	RAMP, LANDING & STAIR DETAILS
24	RAMP, LANDING & STAIR DETAILS
25	-

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NEWBERG SCHOOL DISTRICT

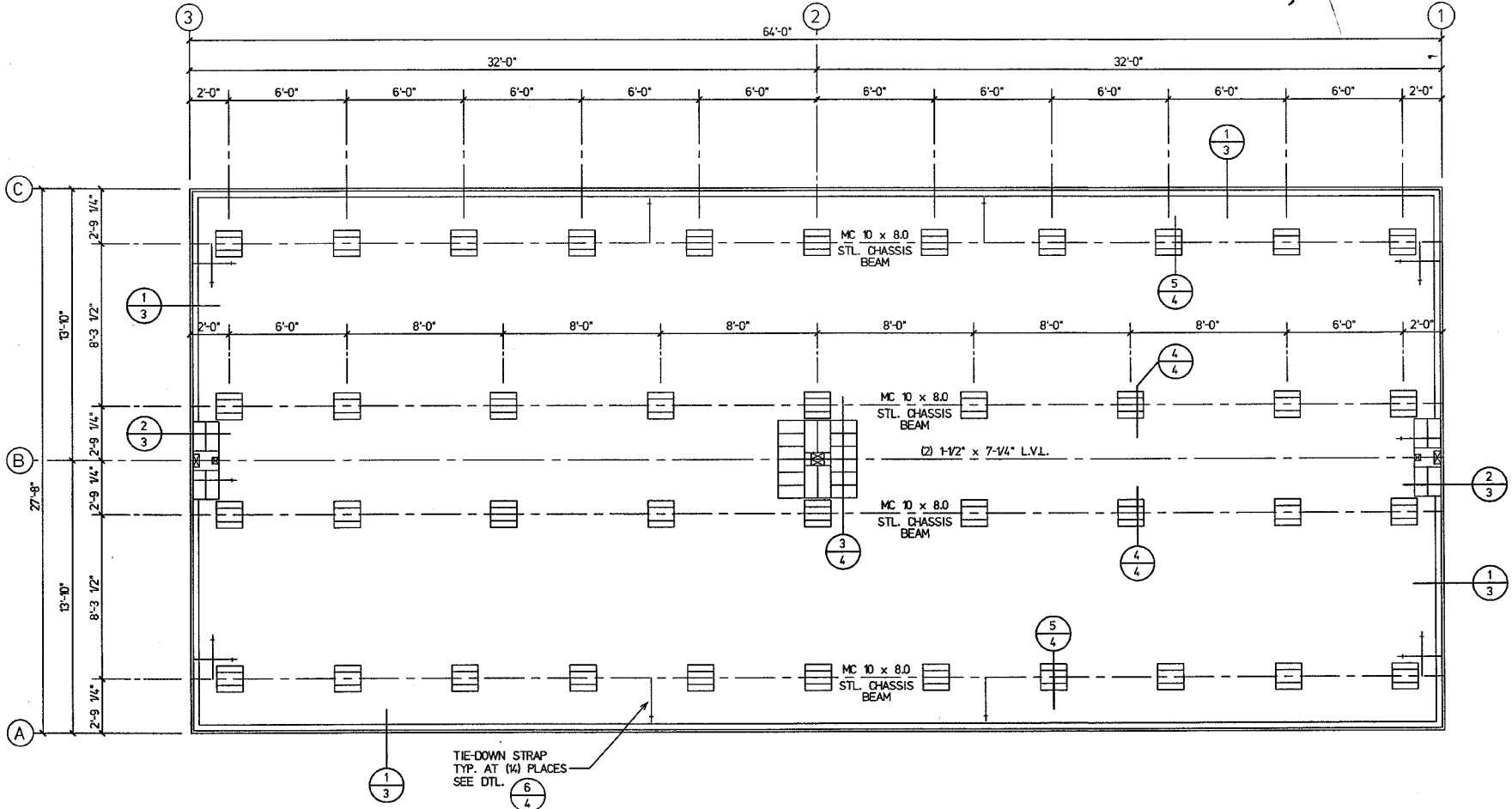
NEWBERG, OREGON

FOUNDATION PLAN



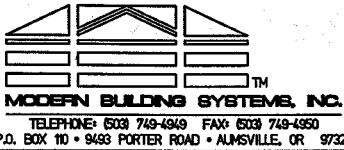
SHEET **2** OF 18

2001-KM-47-1



FOUNDATION PLAN

SCALE: 3/16" = 1'-0"

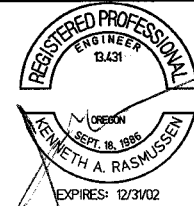


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28' x 64' MODULAR CLASSROOM NEWBERG SCHOOL DISTRICT

NEWBERG , OREGON

FOUNDATION NOTES AND DETAILS



SHEET **3** OF 18
2001-KM-47-1

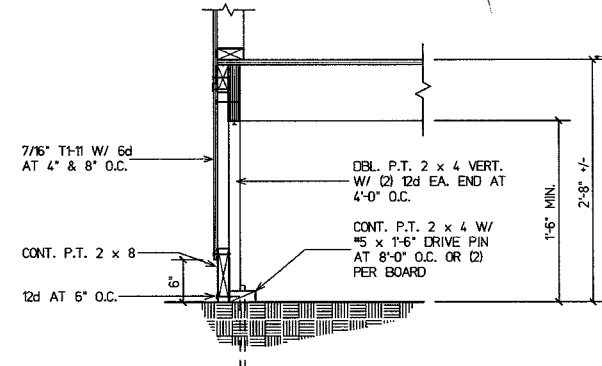
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FOUNDATION NOTES

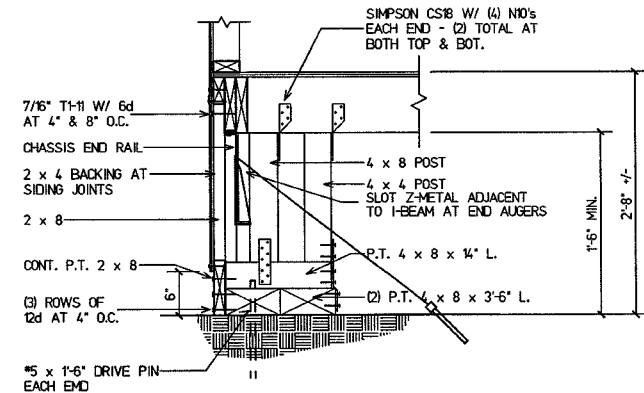
- GENERAL CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS. THIS STRUCTURE SHALL BE ADEQUATELY BRACED FOR WIND OR EARTHQUAKE FORCES AND TEMPORARY FORCES DURING SETTING AND ERECTION UNTIL ALL UNITS HAVE BEEN PERMANENTLY ATTACHED THERETO.
- DESIGN LOADS:

ROOF DEAD LOAD	12 PSF
ROOF SNOW LOAD	25 PSF
FLOOR DEAD LOAD	10 PSF
FLOOR LIVE LOAD	40 PSF
WIND LOAD = 80 MPH	EXP. B
SEISMIC ZONE	3
- EXCEPT AS NOTED, DIMENSION LUMBER FOR FOUNDATIONS SHALL BE HEM-FIR, NO. 2 & BETTER. TREATED LUMBER SHALL BE PRESSURE TREATED IN ACCORDANCE WITH AWPS STANDARD C-2 TO A MINIMUM RETENTION OF .40 PCT. AT PIECES IN CONTACT WITH GROUND. SAWN END GRAIN SHALL BE FIELD TREATED WITH 2% MIN. CONCENTRATION COPPER NAPHTHENATE. TREATED PLATE STOCK SHALL BE GOOD QUALITY AND SHALL NOT CONTAIN EXCESSIVE SPLITS, CHECKS OR WANE.

2 x 4 FRAMING SHALL BE HEM-FIR, STANDARD OR BETTER, TREATED 2 x 4 FRAMING SHALL MEET THE REQUIREMENTS SPECIFIED ABOVE.
- ALL NAILS TO BE GALVANIZED.
- VENT CRAWL SPACE 1 SQ. FT. PER 150 SQ. FT. OF FLOOR AREA OR AS SHOWN. INSTALL 6 MIL. VAPOR BARRIER.
- CONNECT STORM WATER FROM ROOF GUTTERS AND DOWNSPOUTS AND DIRECT AWAY FROM BUILDING PAD TO AN APPROVED DRAINAGE SYSTEM.



1 PERIMETER FOOTING
SCALE: 3/4" = 1'-0"



2 END WALL COLUMN FOOTING
SCALE: 3/4" = 1'-0"

rnewstcl.dwg



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NEWBERG SCHOOL DISTRICT

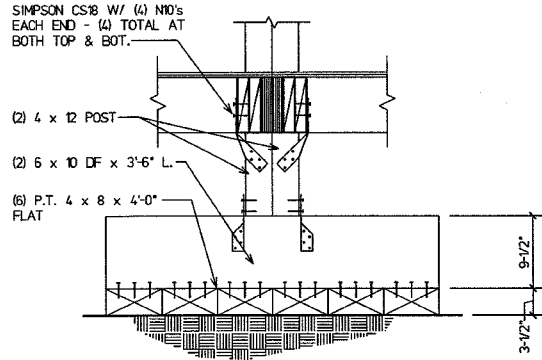
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FOUNDATION DETAILS



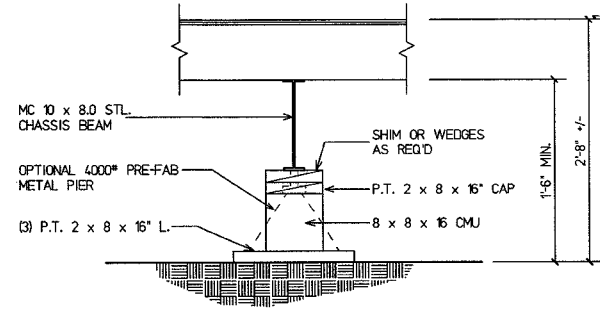
SHEET **4** OF 18

2001-KM-47-1



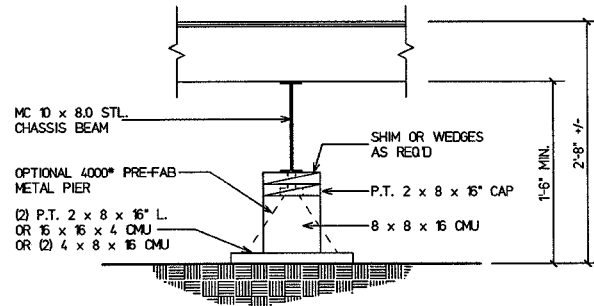
3 COLUMN FOOTING

SCALE: 3/4" = 1'-0"



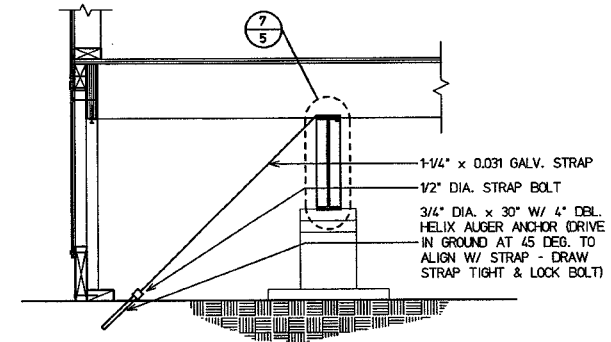
5 EXTERIOR CHASSIS BEAM FOOTING

SCALE: 3/4" = 1'-0"



4 INTERIOR CHASSIS BEAM FOOTING

SCALE: 3/4" = 1'-0"



6 TIE-DOWN STRAP

SCALE: 1" = 1'-0"

rwesd.cb



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28' x 64' MODULAR CLASSROOM NEWBERG SCHOOL DISTRICT

NEWBERG, OREGON

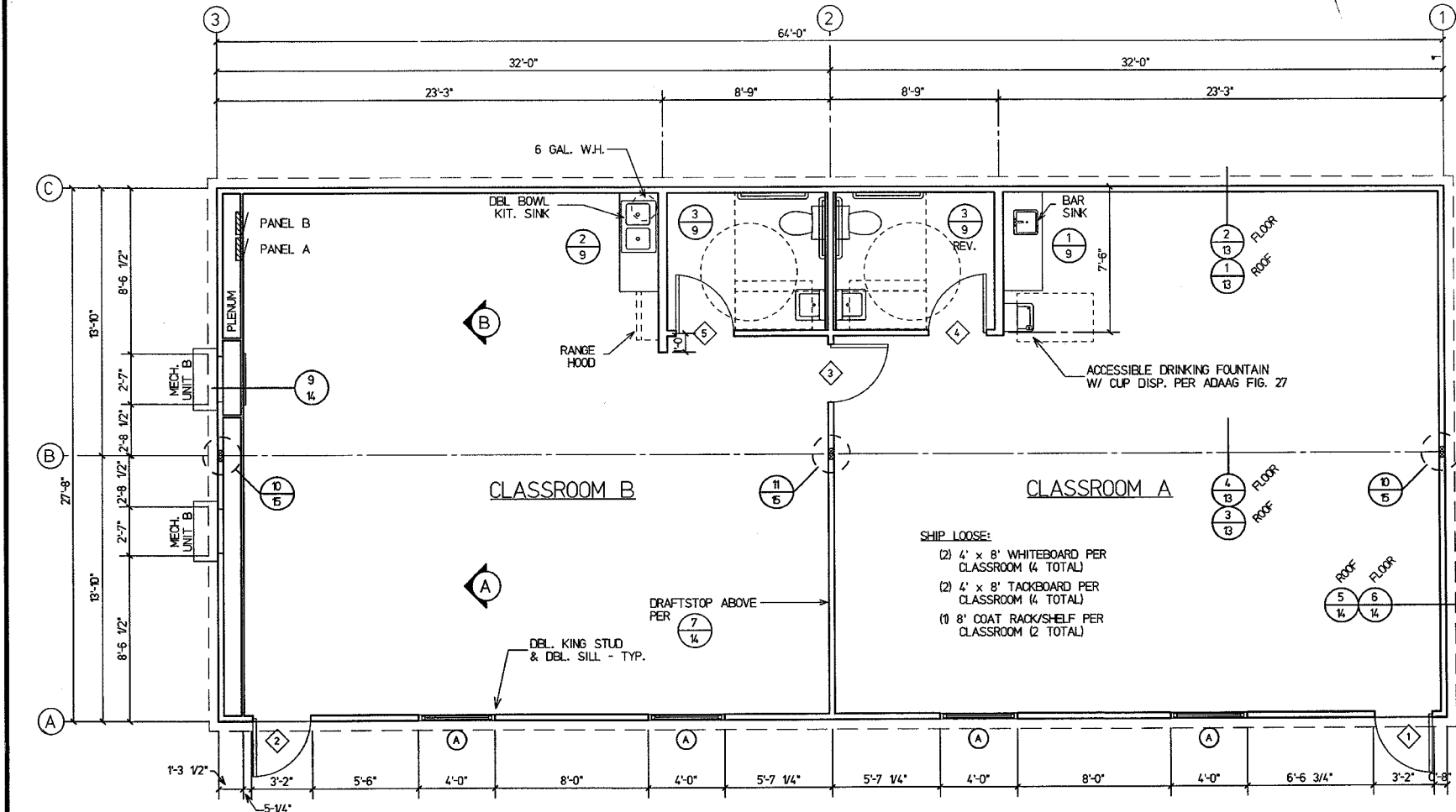


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FLOOR PLAN



FLOOR PLAN

SCALE: 3/16" = 1'-0"



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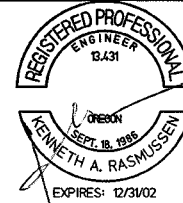
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JGM 9/17/01 CUST. 8,10

28' x 64' MODULAR CLASSROOM NEWBERG SCHOOL DISTRICT

NEWBERG, OREGON

NOTES AND CALCULATIONS



OF 18

8

SHEET

2001-KM-47-1

FLOOR CONSTRUCTION

FINISH: SHEET VINYL AT RESTROOMS, BALANCE BY OTHERS

BASE: 6" VINYL AT RESTROOMS, BALANCE BY OTHERS

UNDERLAYMENT: 3/8" CCP THRU-OUT

SUBFLOOR: 5/8" APA RATED T&G SHEATHING (ACTS AS 1 PERM MAX. VAPOR BARRIER)

JOISTS: 2 x 8 DF #2 & BTR AT 16" O.C. DBL. 2 x 8 AT ENDS

RIM JOISTS: 1-1/2" x 7-1/4" CONTIN. LAMINATED LUMBER (NER 126 OR ICBO 3575)

OFFSET RIMS: 2 x 4

INSULATION: R-11 U (CELLULOSE)

BOTTOM CLOSURE: FS-25

EXTERIOR WALL CONSTRUCTION

SIDING: 7/16" T-11 W/ 8" VERT. GROOVES

SHEATHING: NONE

STUDS: 2 x 4 DF STUD GRADE AT 16" O.C.

INSULATION: R-3 U (FIBERGLASS BATTS)

INTERIOR FINISH: 5/8" V.C. GYP. BD. (ACTS AS 1 PERM MAX. VAPOR BARRIER) F.R.P. OVER WATER RESIST. GYP. BD. AT RESTROOMS (ENTIRE WALLS)

SKIRT: MATCH SIDING

INTERIOR WALL CONSTRUCTION

STUDS: 2 x 4 HF STD & BTR AT 16" O.C. (U.N.O.)

INTERIOR FINISH: 5/8" V.C. GYP. BD. F.R.P. OVER WATER RESIST. GYP. BD. AT RESTROOMS (ENTIRE WALLS)

RESTROOM CEILING NOTES

HARD LID CEILING: 5/8" GYP. BD. W/ F.R.P.

CLASSROOM CEILING NOTES

CEILING HEIGHT: 8'-0" (U.N.O.)

SUSPENDED T-GRID W/ ACOUSTIC TILE (REF: MFR'S INSTALLATION DATA)

INSTALL SUSPENDED CEILING PER UBC STD 25-2

ATTACH LIGHT FIXTURES TO CEILING GRID W/ (4) #6 x 1" STSM SCREWS

ATTACH HVAC DIFFUSERS TO CEILING GRID W/ (4) #6 x 1" STSM SCREWS

LIGHT FIXTURES SHALL BE INDEPENDENTLY SUPPORTED BY 12 GA. WIRES AT EACH CORNER

ROOF CONSTRUCTION

ROOFING: 25 YR. FIBERGLASS COMP. SHINGLES OVER (2) LAYERS 15# FELT (MIN. CLASS B)

MOP IN 1st 2'-0" INSIDE WALL LINE AT EAVE

SHEATHING: 7/16" O.S.B. (24/16)

FRAMING: 2 x 8 DF #1 & BTR AT 24" O.C.

INSULATION: R-21U (CELLULOSE) W/ FS-25 (ACTS AS 1 PERM MAX. VAPOR BARRIER)

BEAMS: 1-1/2" x 24" x 2.0E CONTINUOUS LAMINATED VENEER LUMBER (NER 126 OR ICBO 3575)

OVERHANG: 6"

ROOF PITCH: 2 IN 12

RIDGE VENT

EXTERIOR NOTES

CORNER BATTS: 1/2" x 3-1/2" HARDBOARD

MOD BATTS: 1/2" x 5-1/2" HARDBOARD

FASCIA: 1/2" x 5-1/2" HARDBOARD

RAKE: 1/2" x 5-1/2" HARDBOARD

GUTTERS: 4" K-LINE PREFINISHED

DOWNSPOUTS: 2" x 3" RECT. PREFINISHED

DATA LIST

SQ. FOOTAGE:	1770 S.F.
EXT. WALL LIN. FOOTAGE:	184 L.F.
INT. WALL LIN. FOOTAGE:	60 L.F.
PROJECTED ROOF AREA:	1890 S.F.

OCCUPANT LOAD CALCULATION

CLASSROOMS	(1638 S.F.)/(20)	= 82
RESTROOMS	(132 S.F.)	= 0
TOTAL	1770 S.F.	82

OUTSIDE AIR CALCULATION

o MECHANICAL UNIT A:		
CLASSROOM	(25 OCC)(15 CFM)	= 375
RESTROOM	(1 FIXT)(50 CFM)	= 50
TOTAL		= 425
o MECHANICAL UNIT B:		
CLASSROOM	(25 OCC)(15 CFM)	= 375
RESTROOM	(1 FIXT)(50CFM)	= 50
TOTAL		= 425



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28' x 64' MODULAR CLASSROOM
NEWBERG SCHOOL DISTRICT

NEWBERG, OREGON

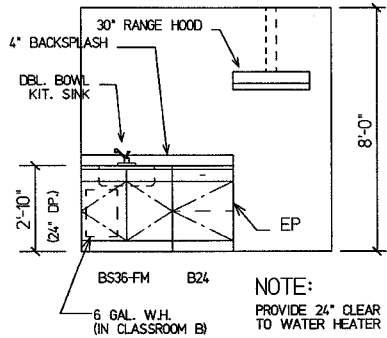
EXTERIOR AND INTERIOR ELEVATIONS



SHEET **9** OF 19

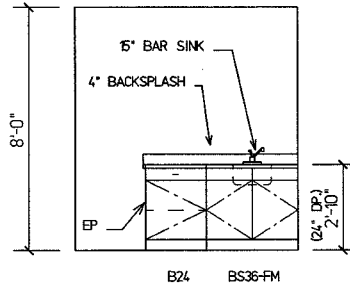
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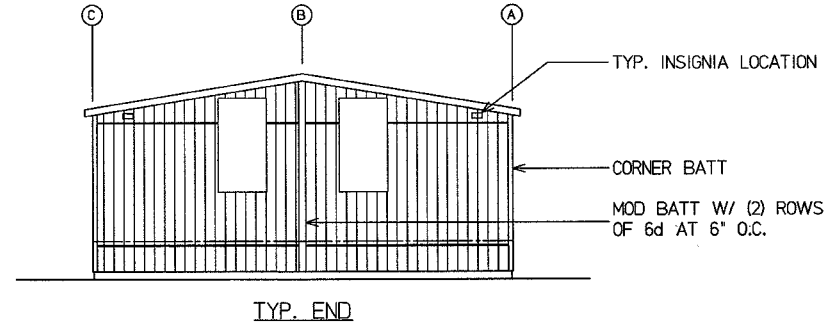


NOTE:
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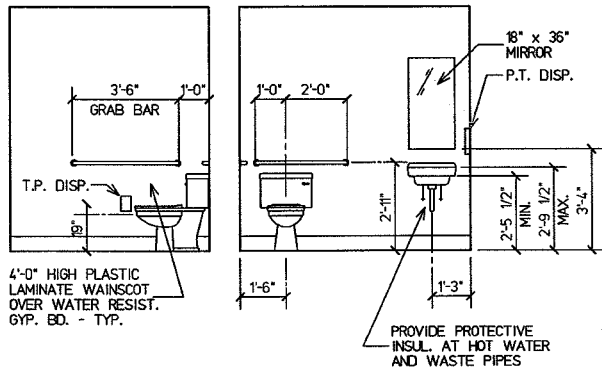
1 TYP. ELEVATION
 SCALE: 1/4" = 1'-0"



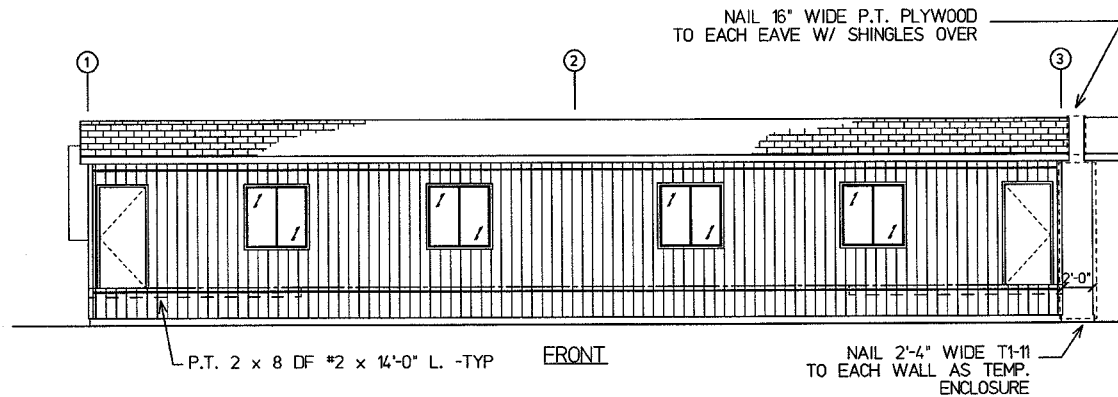
2 TYP. ELEVATION
 SCALE: 1/4" = 1'-0"



TYP. END



3 TYP. RESTROOM ELEVATION
 SCALE: 1/4" = 1'-0"



EXTERIOR ELEVATIONS
 SCALE: 1/8" = 1'-0"



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NEWBERG SCHOOL DISTRICT

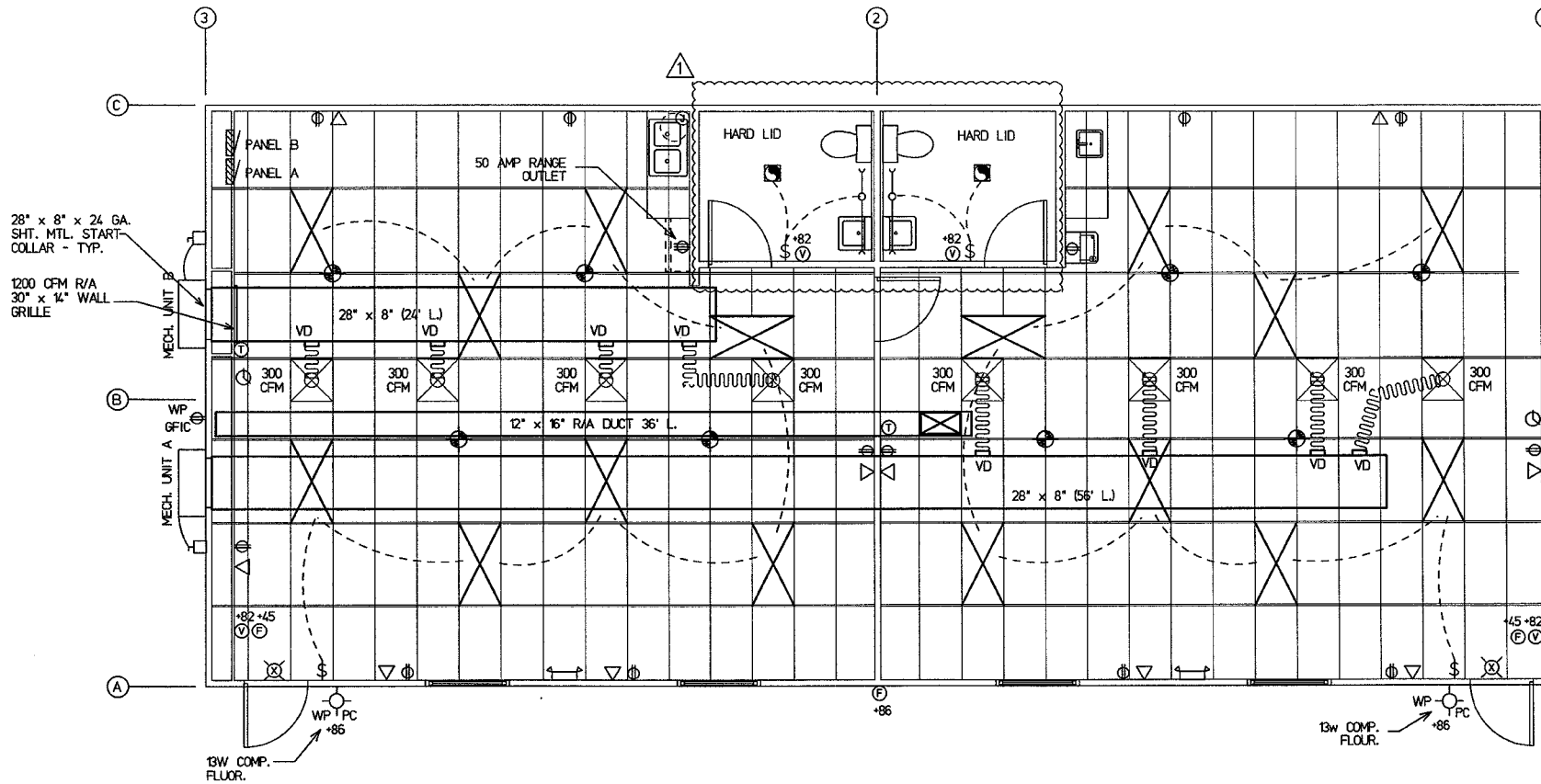
NEWBERG, OREGON

REFLECTED CEILING / ELECTRICAL / MECHANICAL PLAN



SHEET **10** OF 18
2001-KM-47-1

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REFLECTED CEILING/ELEC/MECH PLAN

SCALE: 3/16" = 1'-0"



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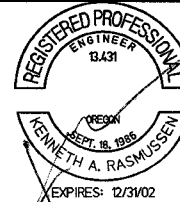
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28' x 64' MODULAR CLASSROOM
NEWBERG SCHOOL DISTRICT

NEWBERG, OREGON

ELECTRICAL / MECHANICAL NOTES AND SCHEDULES



SHEET 11 OF 18

2001-KM-47-1

SYMBOL LEGEND

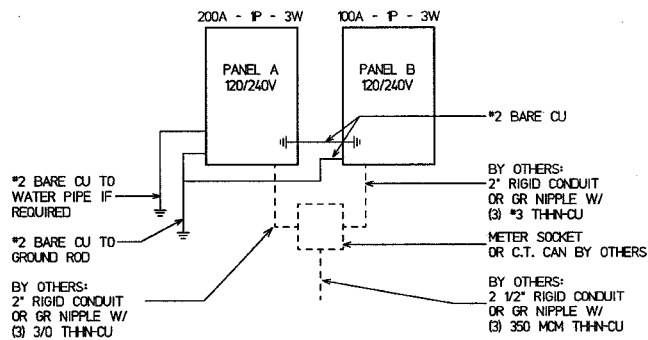
- ☒ 2' x 4' FLUORESCENT LAY-IN FIXTURE
(3) TUBE 32W T8 W/ ELECT BALLAST
- ☒ 2' x 2' LAY-IN SUPPLY DIFFUSER
- ☒ 2' x 2' LAY-IN RETURN GRILLE
- ☒ CEILING MOUNTED EXHAUST FAN (110 CFM)
- ☒ WALL MOUNTED LIGHT FIXTURE
PC = PHOTOCELL
WP = WEATHERPROOF
- ⊕ DUPLEX RECEPTACLE AT 17" A.F.F. (I.N.O.)
GFI = GROUND FAULT INTERRUPTER
WP = WEATHERPROOF
- ⊕ 220v RANGE OUTLET
- \$ SINGLE POLE SWITCH
- \$ THREE-WAY SWITCH
- ⊙ JUNCTION BOX
- ⊙ FIRE ALARM (RACEWAY ONLY)
- ⊙ VISUAL ALARM (RACEWAY ONLY)
+82" TO CENTERLINE
- ⊙ THERMOSTAT AT 52" A.F.F. TO CENTERLINE
- ▽ PHONE OUTLET - CONDUIT DOWN (RACEWAY ONLY)
- ⊙ CLOCK OUTLET - J-BOX AT +88" W/ CONDUIT
DOWN (RACEWAY ONLY)
- ☐ DISCONNECT
- ☒ CEILING MOUNTED EXIT LIGHT W/ BATTERY
BACK-UP AND INTERNATIONAL SYMBOL OF
ACCESSIBILITY
- ☒ EMERGENCY LIGHT W/ BATTERY BACK-UP
- ☒ CEILING SEISMIC BRACE

ELECTRICAL NOTES

- CONDUIT:
 - METALLIC AND M.C. CABLE
 - NON-METALLIC SHEATHED CABLE

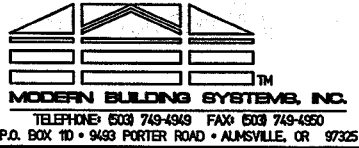
MECHANICAL NOTES

- MECHANICAL UNIT(S):
 - (2) 3.0 TON WALL HUNG HEAT PUMPS
WITH 10 kw ELECTRICAL HEAT
- THERMOSTAT(S): WR IF-95
- DUCT MATERIAL:
 - 1' FIBERGLASS W/ 26 GA. SHEET
METAL ROUNDS
- ECONOMISER(S):
 - ☐ ECONOMISER(S) AND CONTROLS
 - COMMERCIAL ROOM VENTILATOR
 - ☐ NO ECONOMISER(S)



ONE-LINE DIAGRAM

NO SCALE



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**28' x 64' MODULAR CLASSROOM
 NEWBERG SCHOOL DISTRICT**

NEWBERG, OREGON

ELECTRICAL PANEL AND LOAD CALCULATION



SHEET 12 OF 18
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ELECTRICAL LOAD WORKSHEET

PROJECT: NEWBERG S.D. JOB NO.: 2001-47-1 DATE: 09/15/2001
 SQ. FOOTAGE: 1770 OCCUPANCY: E

PANEL: A MAIN SUB VOLTAGE: 120V 240 AMPS: 200 PHASE: 1

ITEM	CONNECTED LOAD	CODE FACTOR		CALCULATED LOAD
LIGHTS (per 220-3b)	5310.00	X 1.25	=	6637.50
SIGN CIRCUIT		X 1.25	=	0.00
RECEPTACLES THRU 10K =	0.00	X 1.00	=	0.00
OVER 10K =	0.00	X 0.50	=	0.00
DEDICATED RECEPTACLES		X 1.00	=	0.00
MOTORS & COMP.				
TOTAL MOTOR KVA =		X 1.00	=	0.00
LARGEST MOTOR =		X 0.25	=	0.00
HEATING & AIR COND. (per mfr's specs)				
HP A = 12720.00 HP B = 12720.00 UNIT E =				
COND. A = 6240.00 COND. B = 6240.00 UNIT F =				
TOTAL HEAT/AC LOAD =	37920.00	X 1.00	=	37920.00
TOTAL ADD'L MECH. (see attached)	0.00	X 1.00	=	0.00
APPLIANCES QTY. =		X 1.00	=	0.00
MISCELLANEOUS				
DRINKING FOUNTAIN	500.00	X 1.00	=	500.00
		X 1.00	=	0.00
		X 1.00	=	0.00
		X 1.00	=	0.00
ADDITIONAL MISC. (see attached)	0.00	X 1.00	=	0.00

CONNECTED LOAD	CALCULATED LOAD
TOTAL WATTS = 43730.00	TOTAL WATTS = 45057.50
TOTAL AMPS = 182.21	TOTAL AMPS = 187.74

PANEL A MAIN DIST. SURFACE PLUSH
 SUB-PANEL DATE: 09/15/2001 JOB NO.: 2001-47-1

AMP	CIR NO	P	WATTS	P	CIR NO	AMP	MECHANICAL UNIT
20	1	A	888	12220	A	2	60
20	3	B	999	1	B	1	4
20	5	A	500	6240	A	2	30
20	7	B	1	6240	B	1	1
9	A		12220	A	2	60	
11	B		1	B	1	1	
13	A		6240	A	2	30	
15	B		1	B	1	1	
17	A		1	A	2	30	
19	B		1	B	1	1	
21	A		1	A	2	30	
23	B		1	B	1	1	
25	A		1	A	2	30	
27	B		1	B	1	1	
29	A		1	A	2	30	
31	B		1	B	1	1	
33	A		1	A	2	30	
35	B		1	B	1	1	
37	A		1	A	2	30	
39	B		1	B	1	1	
41	A		1	A	2	30	

TOTAL PANEL WATTS = 40416 168.41 = TOTAL PANEL AMPS

BUSS SIZE = 200 VOLTAGE = 120V 240
 MAIN BREAKER = 200 PHASE = 1

PANEL B MAIN DIST. SURFACE PLUSH
 SUB-PANEL DATE: 09/15/2001 JOB NO.: 2001-47-1

AMP	CIR NO	P	WATTS	P	CIR NO	AMP	MECHANICAL UNIT
20	1	A	200	1500	A	1	2
20	3	B	800		B	4	20
20	5	A	640		A	6	
50	7	B	4800		B	8	
1	9	A	4800		A	10	
11	A				B	12	
13	A				A	14	
15	B				B	16	
17	A				A	18	
19	B				B	20	
21	A				A	22	
23	B				B	24	
25	A				A	26	
27	B				B	28	
29	A				A	30	
31	B				B	32	
33	A				A	34	
35	B				B	36	
37	A				A	38	
39	B				B	40	
41	A				A	42	

TOTAL PANEL WATTS = 11840 49.33 = TOTAL PANEL AMPS

BUSS SIZE = 100 VOLTAGE = 120V 240
 MAIN BREAKER = 100 PHASE = 1

ELECTRICAL LOAD WORKSHEET

PROJECT: NEWBERG S.D. JOB NO.: 2001-47-1 DATE: 09/15/2001
 SQ. FOOTAGE: OCCUPANCY: E

PANEL: B MAIN SUB VOLTAGE: 120V 240 AMPS: 100 PHASE: 1

ITEM	CONNECTED LOAD	CODE FACTOR		CALCULATED LOAD
LIGHTS (per 220-3b)	0.00	X 1.25	=	0.00
SIGN CIRCUIT		X 1.25	=	0.00
RECEPTACLES THRU 10K =	2340.00	X 1.00	=	2340.00
OVER 10K =	0.00	X 0.50	=	0.00
DEDICATED RECEPTACLES		X 1.00	=	0.00
MOTORS & COMP.				
TOTAL MOTOR KVA =		X 1.00	=	0.00
LARGEST MOTOR =		X 0.25	=	0.00
HEATING & AIR COND. (per mfr's specs)				
UNIT A = UNIT C = UNIT E =				
UNIT B = UNIT D = UNIT F =				
TOTAL HEAT/AC LOAD =	0.00	X 1.00	=	0.00
TOTAL ADD'L MECH. (see attached)	0.00	X 1.00	=	0.00
APPLIANCES QTY. =		X 1.00	=	0.00
MISCELLANEOUS				
WATER HEATER	1500.00	X 1.00	=	1500.00
RANGE	8000.00	X 1.00	=	8000.00
		X 1.00	=	0.00
		X 1.00	=	0.00
ADDITIONAL MISC. (see attached)	0.00	X 1.00	=	0.00

CONNECTED LOAD	CALCULATED LOAD
TOTAL WATTS = 11840.00	TOTAL WATTS = 11840.00
TOTAL AMPS = 49.33	TOTAL AMPS = 49.33



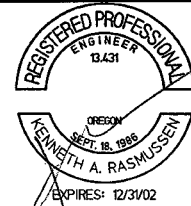
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**28' x 64' MODULAR CLASSROOM
 NEWBERG SCHOOL DISTRICT**

NEWBERG, OREGON

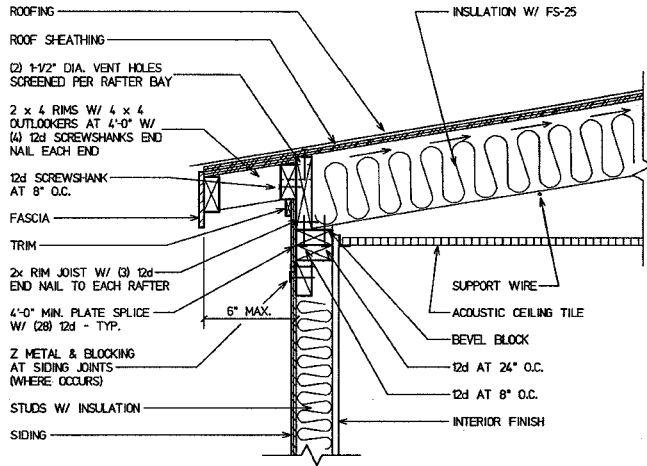


SHEET 13 OF 18

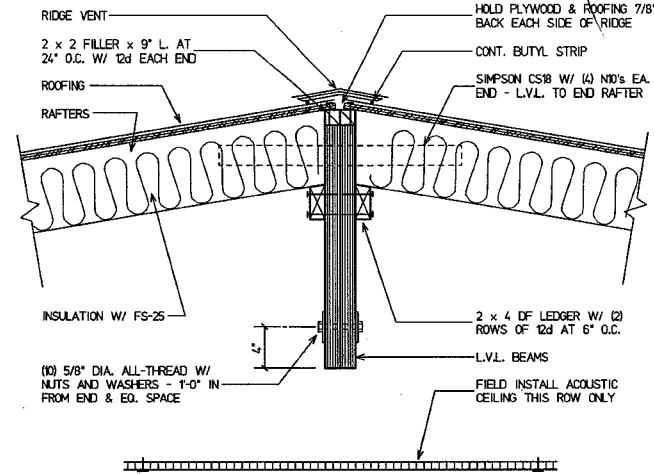
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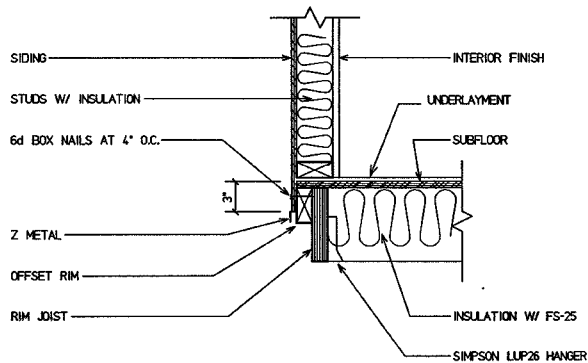
DETAILS



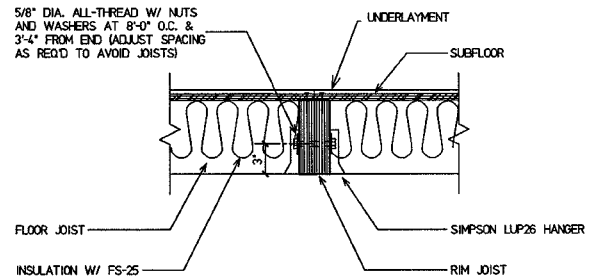
1 TYP. SIDEWALL EAVE DETAIL
 SCALE: 1" = 1'-0"



3 ROOF MARRIAGE DETAIL
 SCALE: 1" = 1'-0"



2 TYP. SIDEWALL FLOOR DETAIL
 SCALE: 1" = 1'-0"



4 FLOOR MARRIAGE DETAIL
 SCALE: 1" = 1'-0"

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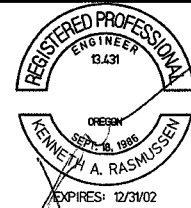
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28' x 64' MODULAR CLASSROOM
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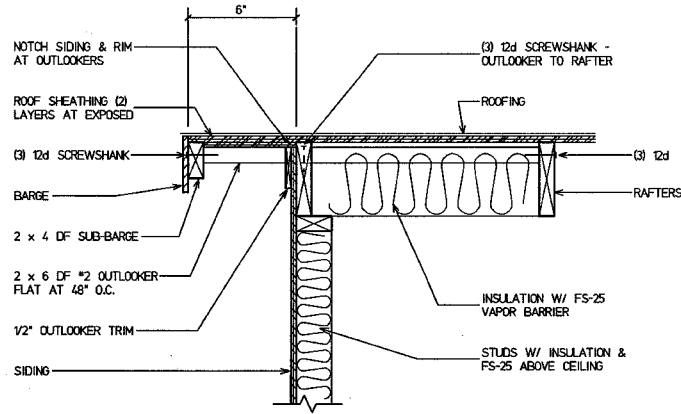
NEWBERG, OREGON

DETAILS

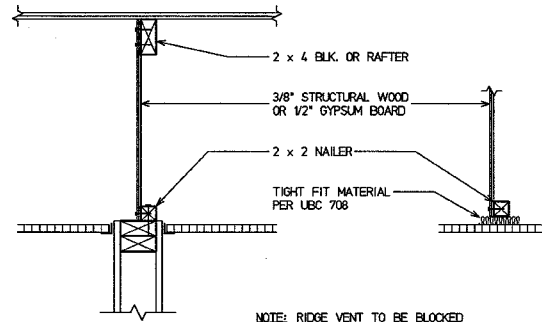


SHEET 14 OF 18

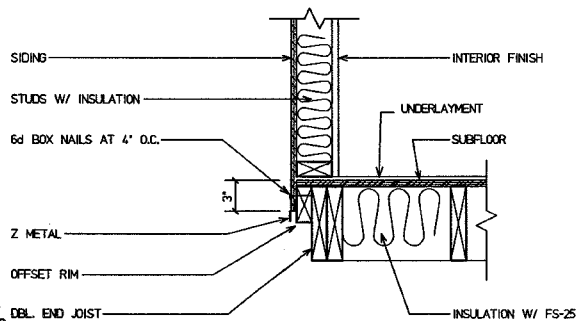
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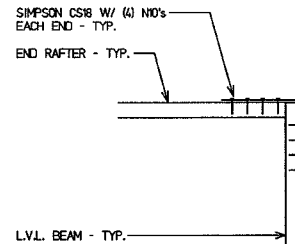
5 GABLE END DETAIL
 SCALE: 1" = 1'-0"



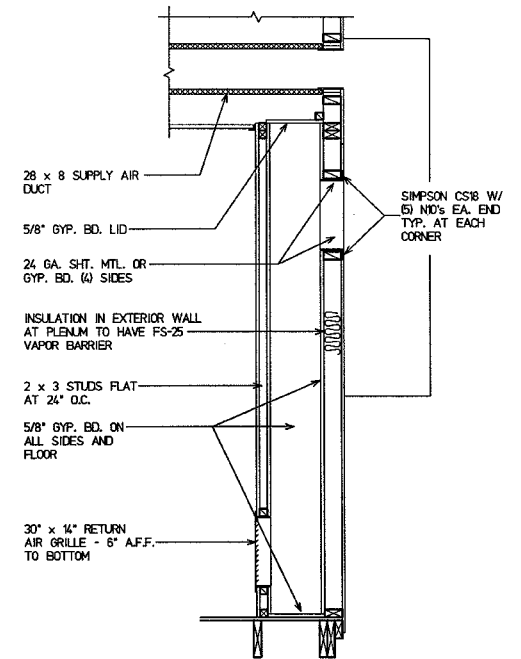
7 DRAFTSTOP DETAIL
 SCALE: 1" = 1'-0"



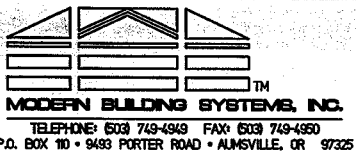
6 TYP. ENDWALL FLOOR DETAIL
 SCALE: 1" = 1'-0"



8 PLAN VIEW AT CORD TIE
 SCALE: 1" = 1'-0"



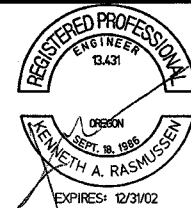
9 PLENUM DETAIL
 SCALE: 1/2" = 1'-0"



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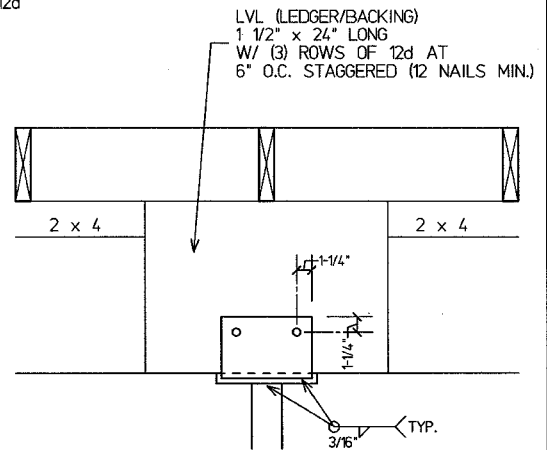
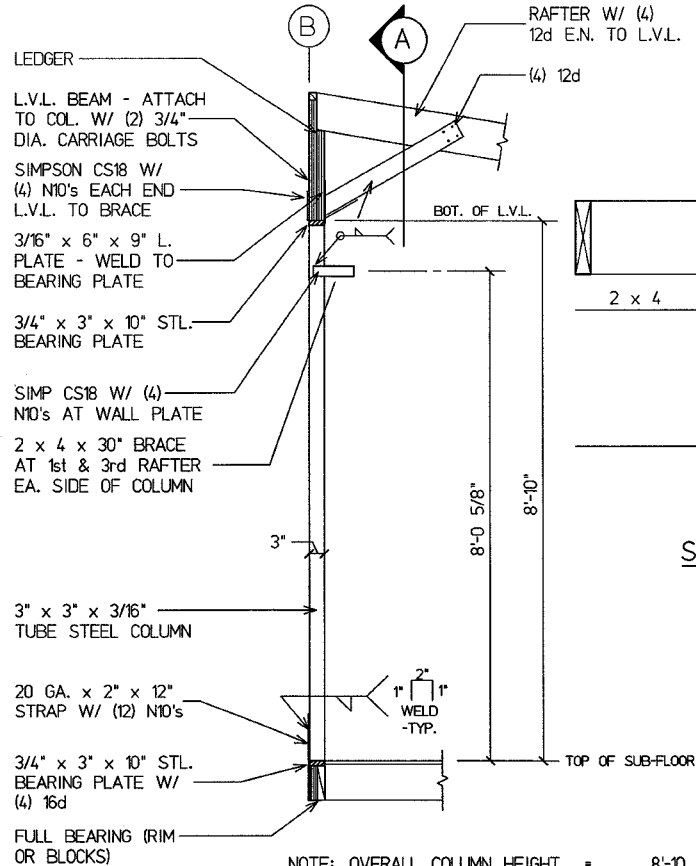
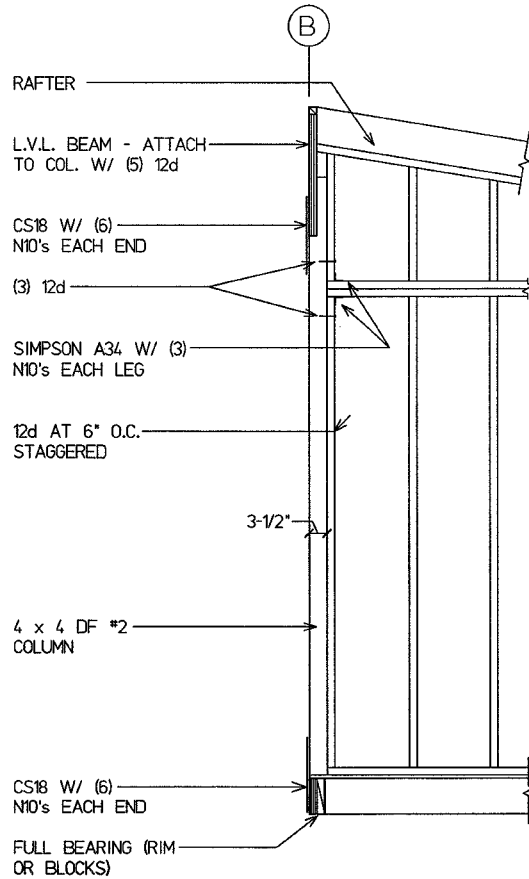
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SHEET 15 OF 19
 2001-KM-47-1

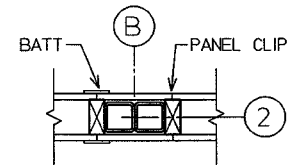
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DETAILS



SECTION A

SCALE: 1" = 1'-0"



PLAN VIEW

SCALE: 1" = 1'-0"

NOTE: OVERALL COLUMN HEIGHT = 8'-10 3/4"
 (W/ PLATES)
 TOTAL NO. OF COLUMNS REQ'D: = 2

10 END WALL COL. DETAIL SCALE: 1/2" = 1'-0"

11 CENTER COLUMN DETAIL SCALE: 1/2" = 1'-0"



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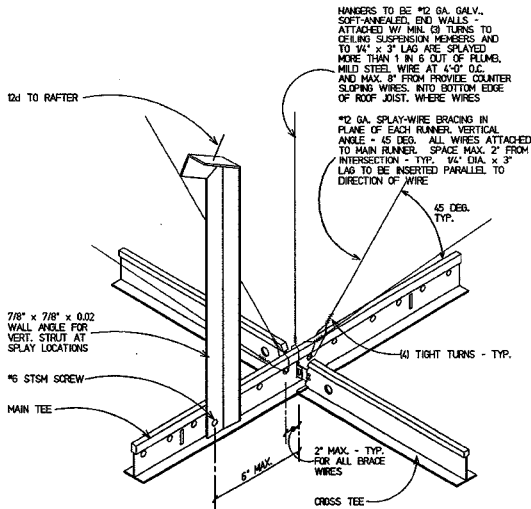
DETAILS



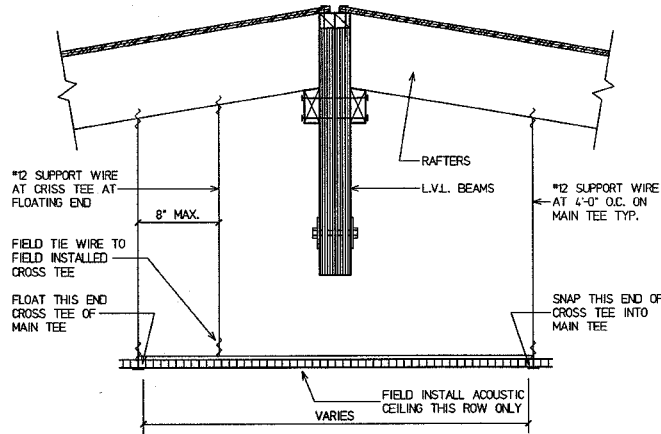
SHEET **16** OF 18

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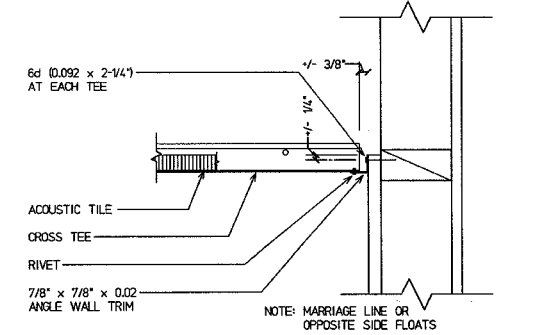
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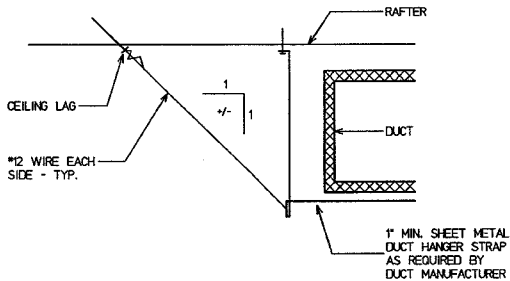
12 CEILING BRACING DETAIL
16 NO SCALE



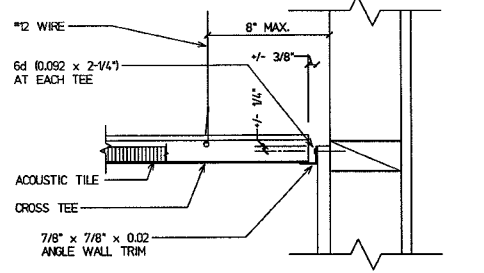
14 CEILING GRID AT MARRIAGE
16 NO SCALE



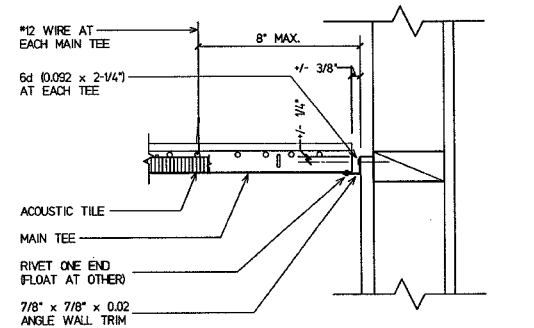
16 CEILING GRID AT SIDE WALL
16 FIXED EDGE NO SCALE



13 DUCT SUPPORT
16 NO SCALE



15 CEILING GRID AT SIDE WALL
16 FLOATING EDGE NO SCALE



17 CEILING GRID AT END WALL
16 NO SCALE

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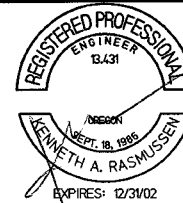
REV.# BY: DATE: PER: SHEETS:

JGM 9/12/01 - PRELIM.
 JGM 9/13/01 - SUBMIT.

**28' x 64' MODULAR CLASSROOM
 NEWBERG SCHOOL DISTRICT**

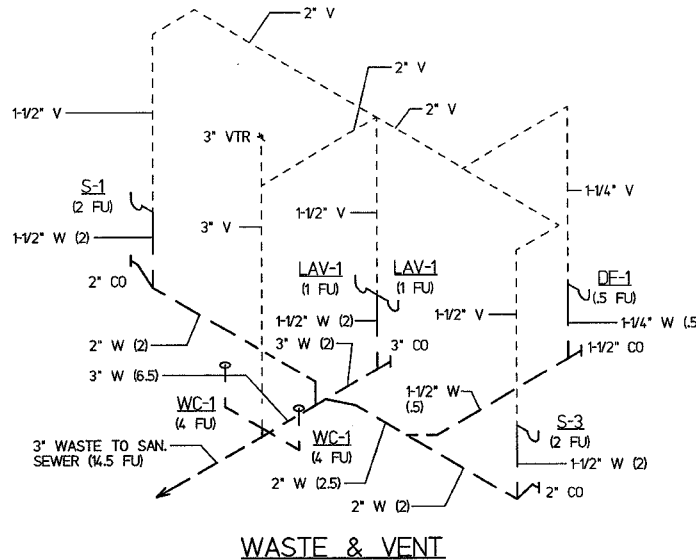
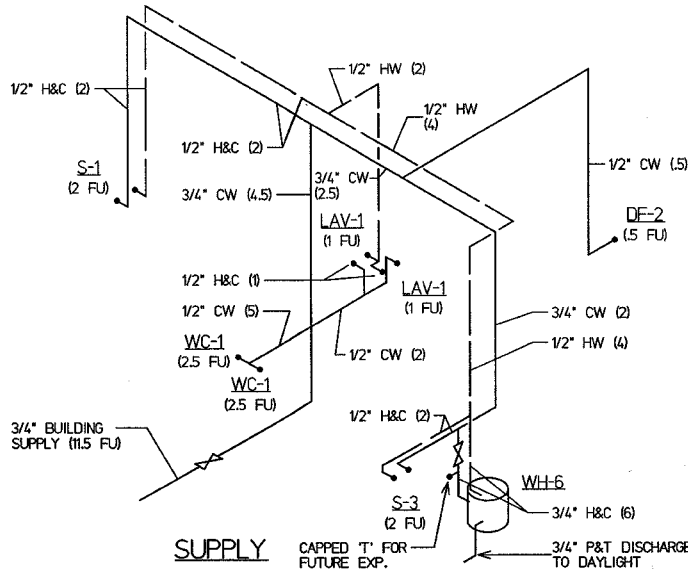
NEWBERG, OREGON

PLUMBING DIAGRAMS



SHEET **17** OF 18

2001-KM-47-1



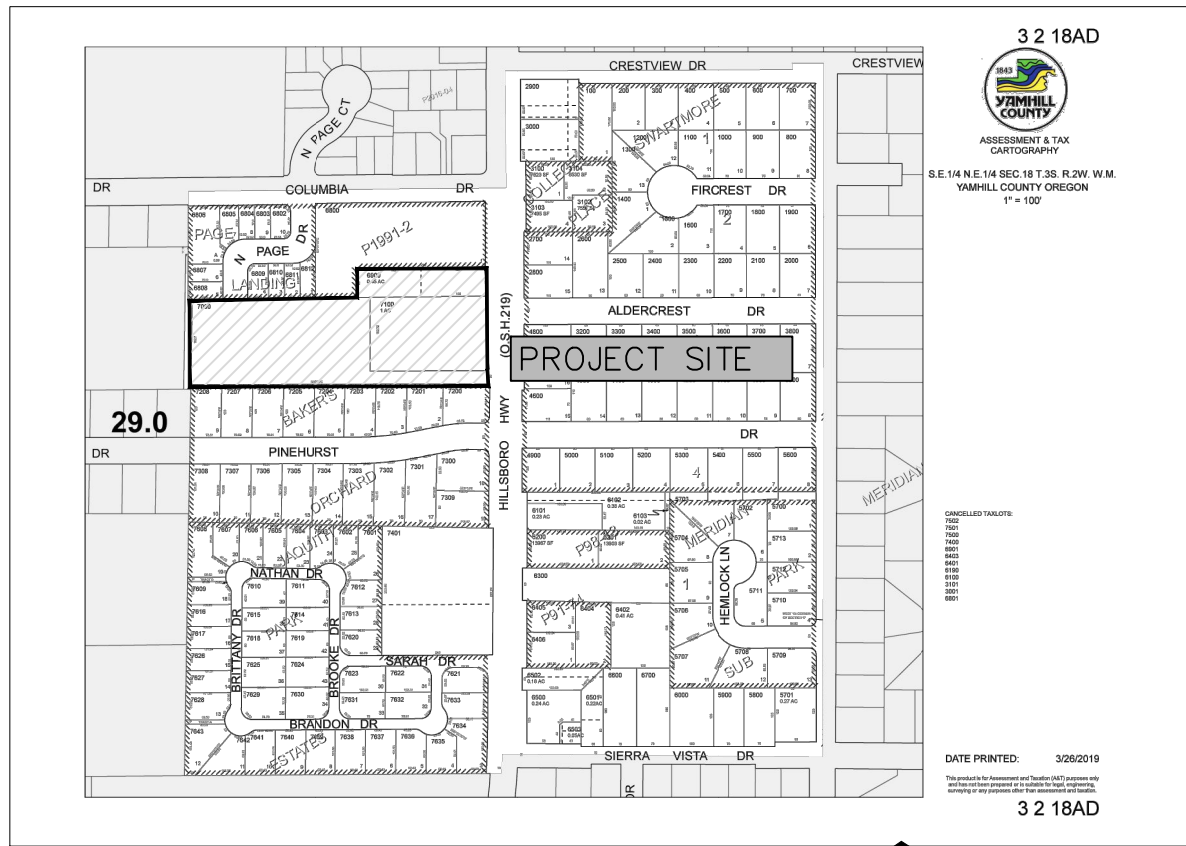
PLUMBING FIXTURE SCHEDULE

WATER CLOSETS							
REFERENCE	DESCRIPTION	STYLE	GPF	MANUFACTURER	MODEL	CONNECTION	VALVE
WC-1	ACCESSIBLE W.C.	TANK	16	GERBER	AQUA-SAVER 21-718	3" CLOSET, 1/2" CW	
LAVATORIES							
REFERENCE	DESCRIPTION	STYLE	GPM	MANUFACTURER	MODEL	CONNECTION	VALVE
LAV-1	WALL-HUNG LAV		2.5	GERBER	PLYMOUTH 12-314	1/2" CW, 1/2" HW, 1-1/4" W	DELTA 520 LEVER
SINKS							
REFERENCE	DESCRIPTION	STYLE	GPM	MANUFACTURER	MODEL	CONNECTION	VALVE
S-1	HAND/BAR SINK	SS	2.5	POLAR	659 16" x 16"	1/2" CW, 1/2" HW, 2" W	DELTA 100 LEVER
S-3	2-BOWL KIT. SINK	SS	2.5	POLAR	332-8 33" x 22"	1/2" CW, 1/2" HW, 2" W	DELTA 100 LEVER
DRINKING FOUNTAINS							
REFERENCE	DESCRIPTION	STYLE	GPM	MANUFACTURER	MODEL	CONNECTION	ELECT.
DF-2	SINGLE DF	ACCESSIBLE		OASIS	P8AM	1/2" CW, 1-1/4" W	110V CORD/PLUG
WATER HEATERS							
REFERENCE	DESCRIPTION	STYLE	WATTS	MANUFACTURER	MODEL	CONNECTION	ELECT.
WH-6	6 GAL. TANK	ELEC.	2000	RUUD	PEP-6-1	3/4"	110V 16.7A

PLUMBING NOTES

- o PLUMBING DROP MARKERS (ATTACH & SUSPEND BELOW FLOOR JOISTS):
 COLD WATER - BLUE/WHITE
 HOT WATER - RED
 DRAINS - YELLOW
 ROOF DRAIN - WHITE
 VENT - ORANGE
- o STRAP WTR. HTR. TO WALL W/ 26 GA. x 1" SEISMIC STRAP W/ (1) #6 SMST TO TANK AND (1) #3 x 2" BUGLE HEAD SCREW TO WALL EACH END
- o LAVS TO MEET OSPSC 414.2
- o TRAP PRIMERS TO BE MIN. 12" ABOVE FLOOD RIM PER OSPSC 1007.4
- o HANDICAP ACCESSIBLE URINAL CONTROLS NOT TO EXCEED 44" A.F.F.
- o SUPPLY PIPING: TYPE M' CU
- o WASTE PIPING: ABS - DWV
- o SUPPLY PIPE SIZES BASED ON: 100 FT. DEVELOPED LENGTH 46 - 60 PSI

rsmwtdab



VICINTY MAP
NOT TO SCALE
NORTH

3 2 18AD

 ASSESSMENT & TAX
 CARTOGRAPHY
 S.E. 1/4 N.E. 1/4 SEC. 18 T.3S. R.2W, W.M.
 YAMHILL COUNTY OREGON
 1" = 100'
 CANCELLED TAXLOTS:
 7100
 7200
 7300
 7400
 7500
 7600
 7700
 7800
 7900
 8000
 8100
 8200
 8300
 8400
 8500
 8600
 8700
 8800
 8900
 9000
 DATE PRINTED: 3/29/2019
 3 2 18AD

PROJECT CONTACTS

CLIENT:
 CS LEWIS ACADEMY
 1605 N COLLEGE ST
 NEWBERG, OR 97123

ENGINEER:
 YAMHILL LAND DEVELOPMENT SERVICES LLC
 PO BOX 1042
 NEWBERG, OR 97132
 DANIEL DANICIC, PE
 503-476-7702

ENGINEER'S NOTE TO CONTRACTOR:

THE EXISTENCE AND LOCATION OF ANY UNDERGROUND UTILITIES OR STRUCTURES SHOWN ON THESE PLANS ARE OBTAINED BY A SEARCH OF AVAILABLE RECORDS. TO THE BEST OF OUR KNOWLEDGE, THERE ARE NO EXISTING UTILITIES EXCEPT THOSE SHOWN ON THESE PLANS. THE CONTRACTOR IS REQUIRED TO TAKE DUE PRECAUTIONARY MEASURES TO PROTECT THE UTILITY LINES SHOWN ON THESE DRAWINGS. THE CONTRACTOR FURTHER ASSUMES ALL LIABILITY AND RESPONSIBILITY FOR THE UTILITY PIPES, CONDUITS OR STRUCTURES SHOWN OR NOT SHOWN ON THESE DRAWINGS.

THE CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR THE JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY; THAT THIS SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY AND HOLD THE OWNER AND THE ENGINEER HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPT FOR LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER OR THE ENGINEER.

CONTRACTOR SHALL VERIFY ALL CONDITIONS AND DIMENSIONS AND SHALL REPORT ANY DISCREPANCIES TO THE ENGINEER PRIOR TO THE COMMENCEMENT OF WORK.

TITLE PAGE

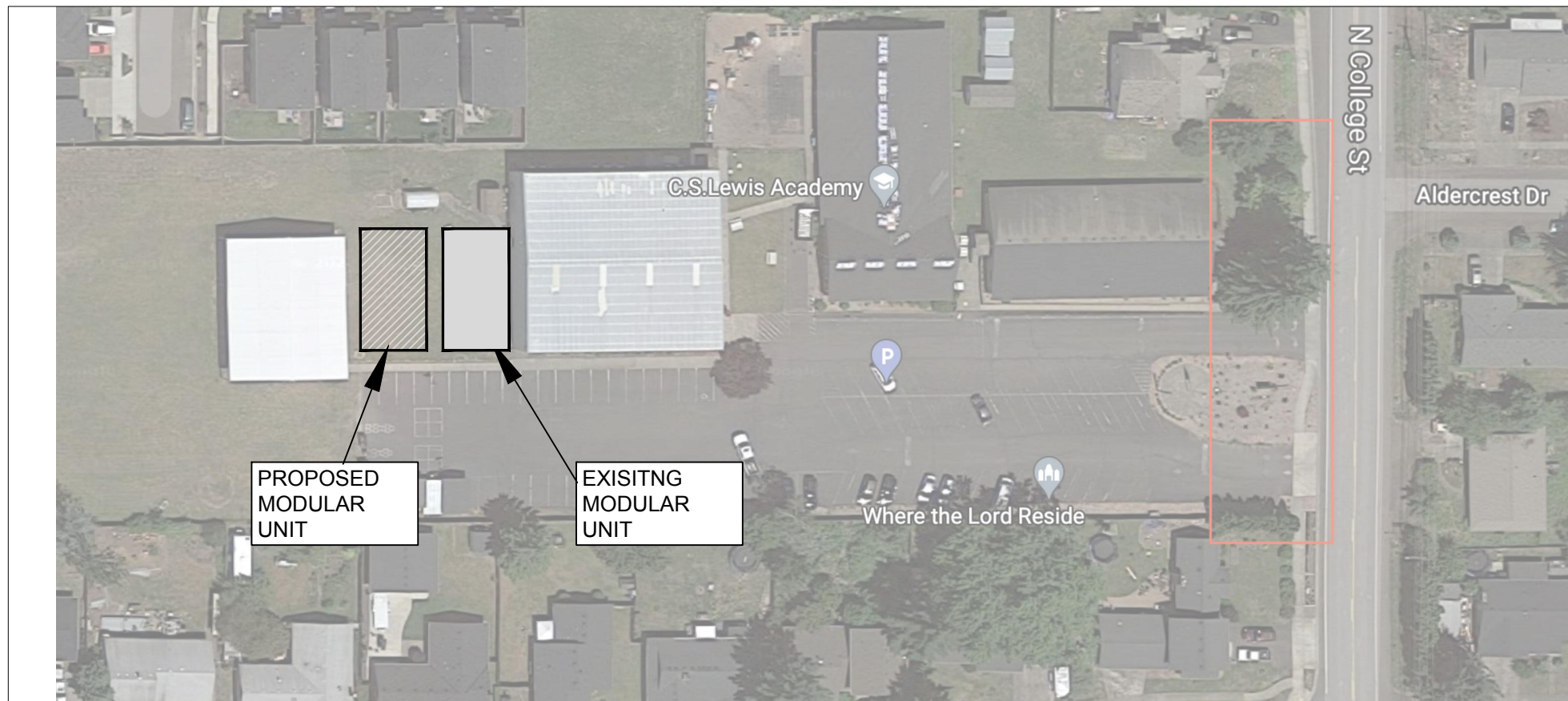
CS LEWIS
 MODULAR CLASSROOM
 INSTALLATION

1605 N COLLEGE ST
 NEWBERG, OR 97132

REVISIONS		DESCRIPTION
NO.	DATE	TYPE 2 APPLICATION
00	03/17/23	

PREPARED BY
 YAMHILL LAND DEVELOPMENT SERVICES LLC
 PO BOX 1042
 NEWBERG, OR 97132
 DANIEL DANICIC, PE
 D.J.DANICIC@GMAIL.COM

SHEET
G1



SITE MAP
NORTH

NOTICE TO EXCAVATORS

ATTENTION: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THE RULES BY CALLING THE CENTER. (NOTE: THE TELEPHONE NUMBER FOR THE OREGON UTILITY NOTIFICATION CENTER IS (503)-232-1987).

POTENTIAL UNDERGROUND FACILITY OWNERS

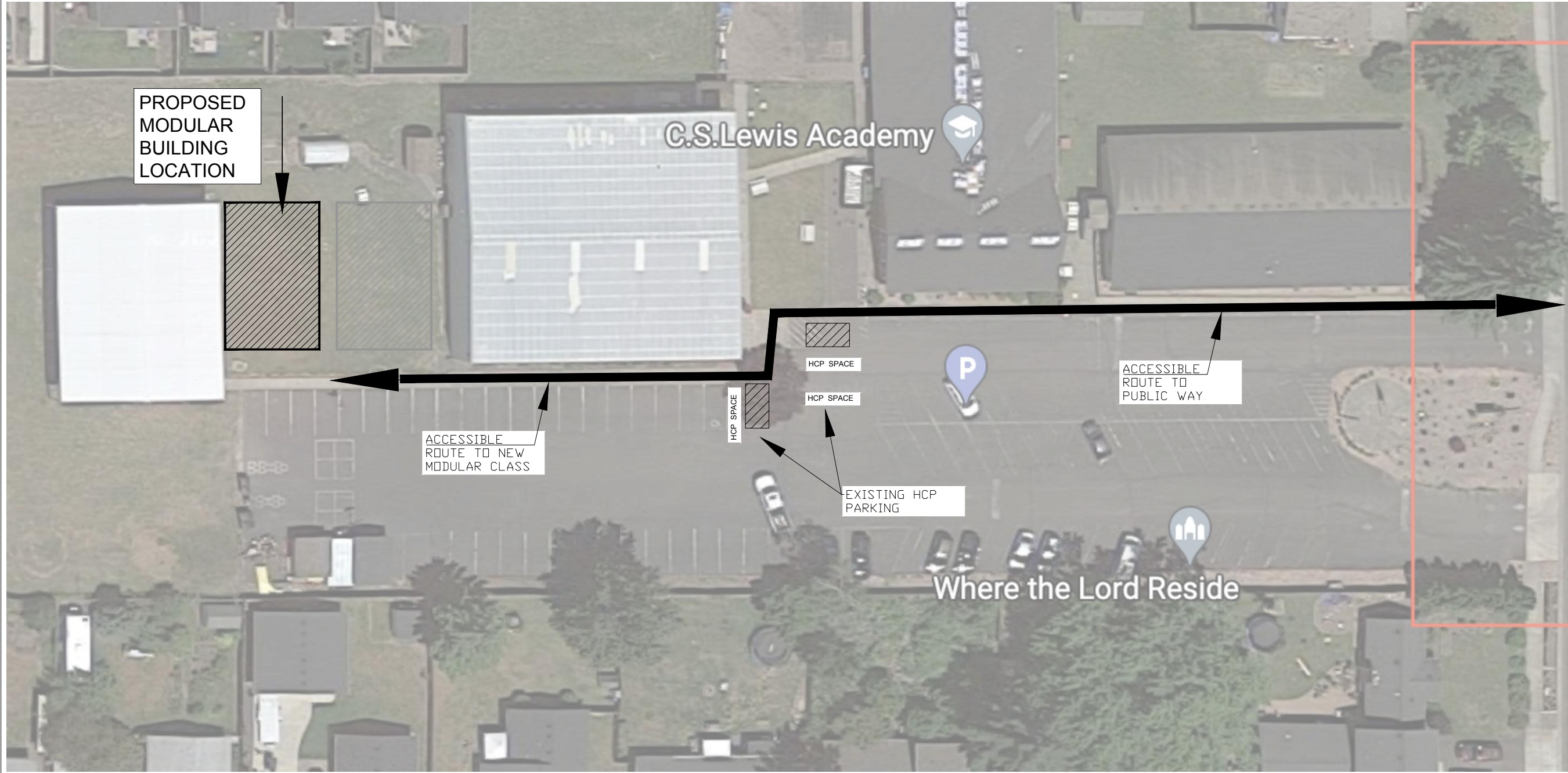
Dig Safely.

Call the Oregon One-Call Center
 DIAL 811 or 1-800-332-2344

SHEET INDEX

- G1 TITLE PAGE
- G2 SITE ACCESS PLAN
- C1 SITE PLAN
- C2 DETAILS
- ESC1 EROSION CONTROL

PRELIMINARY



ACCESSIBILITY ROUTE
NOT TO SCALE



CS LEWIS ACADEMY
1605 N. COLLEGE ST.
NEWBERG, OR 97132

MODULAR BUILDING ADDITION
ACCESSIBILITY ROUTE

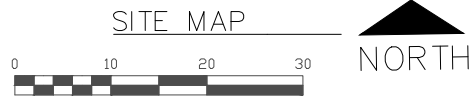
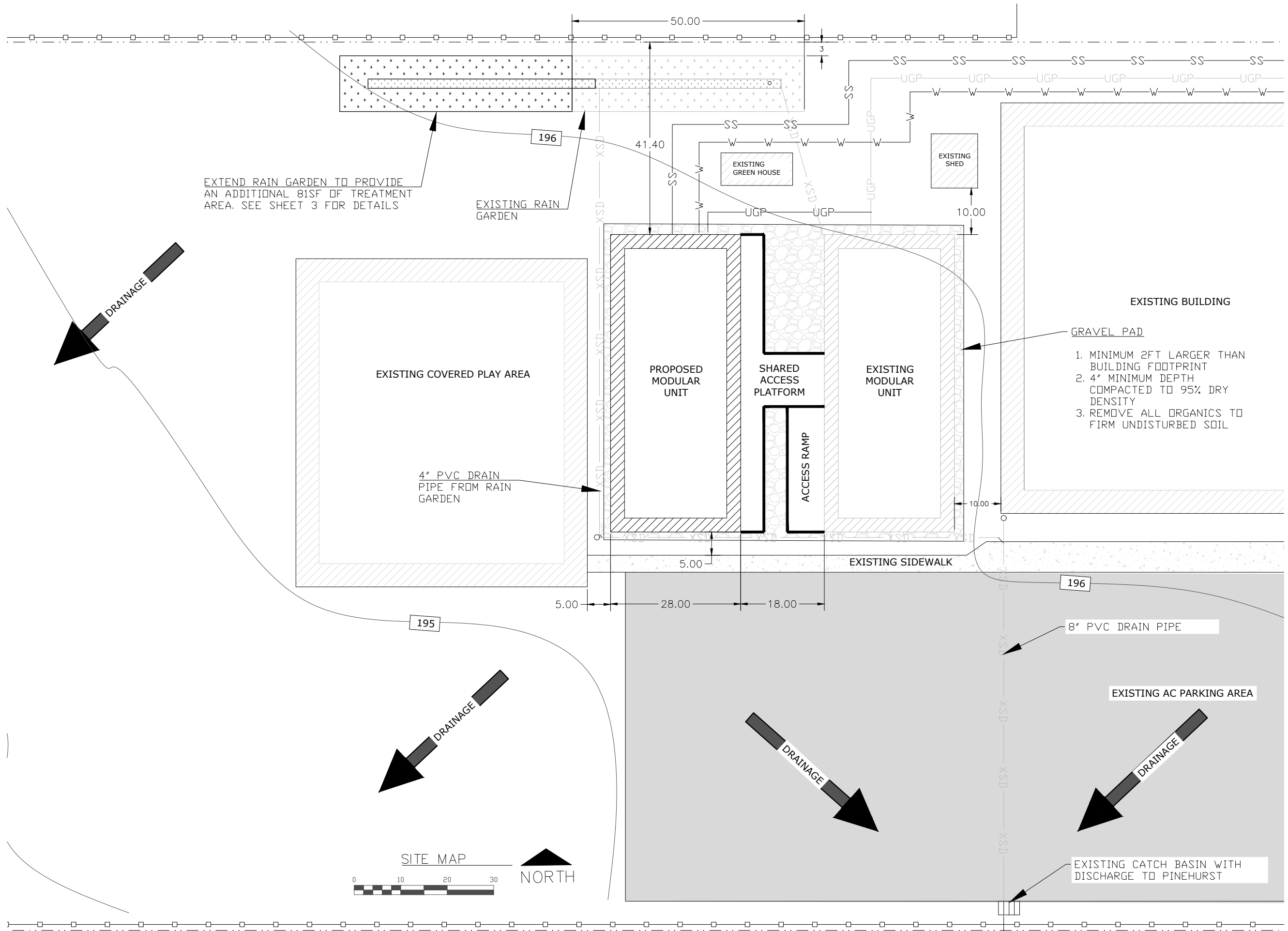
NO.	DATE	DESCRIPTION
00	03/17/23	TYPE 2 APPLICATION

PREPARED BY:
DANIEL DANICIC, PE
YAMHILL LAND DEVELOPMENT SERVICES
PO BOX 1042
NEWBERG, OR 97132
D.DANICIC@YMAIL.COM
503-476-7702

PRELIMINARY

EXPIRES 12/31/23

SHEET
G2



CS LEWIS ACADEMY
1605 N. COLLEGE ST.
NEWBERG, OR 97132

MODULAR BUILDING ADDITION
SITE IMPROVEMENTS

NO.	DATE	DESCRIPTION
00	03/17/23	TYPE 2 APPLICATION

PREPARED BY:
DANIEL DANICIC, PE
YAMHILL LAND DEVELOPMENT SERVICES
PO BOX 1042
NEWBERG, OR 97132
DJD.YLDS@GMAIL.COM
503-476-7702

EXPIRES 12/31/23

SHEET
C1

PRELIMINARY

City of Newberg LIDA Sizing Form

(Include this form with plan submittal)

Project Title: CS Lewis Modular Classroom Addition

Project Address: 1605 N College St. Newberg OR 97132

Project Taxlot/ Taxmap#: _____

Project Location: _____

Contact Name/Title/Company: Steve Wallo, Administrator C.S. Lewis Academy

Phone/e-mail: 503.989.18907

STEP 1: Determine Impervious Area Requiring Treatment

Total Gross Site Area (acres): Pre. Dev. Impervious Area (ft): (X)

Proposed Net New Impervious Area (ft): (PA) Post Dev. Impervious Area (ft): (Y)
 (PA)= (Y) - (X)

STEP 2: Deduct Impervious Area LIDA Credits

Porous Pavement (sq. ft.): (P)

Green Roof (sq. ft.): (G)

Other Credits as approved (sq. ft.): (O)

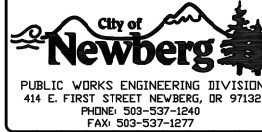
Total Credits (sq. ft.): (C)
 (C)= (P)+(G)+(O)

Impervious Area Requiring Treatment (sq. ft.): (IA)
 (IA)= (PA) - (C)

STEP 3: Size LIDA Facilities for Remaining Impervious Area

	Impervious Area Treated (sq. ft.)	SF, Sizing Factor	LIDA Facility Size (sq. ft.)
Infiltration Planters/ Rain Garden	1792	0.045	80.64
Flow-through Planter		0.060	
Public Flow-through Planter		0.060	

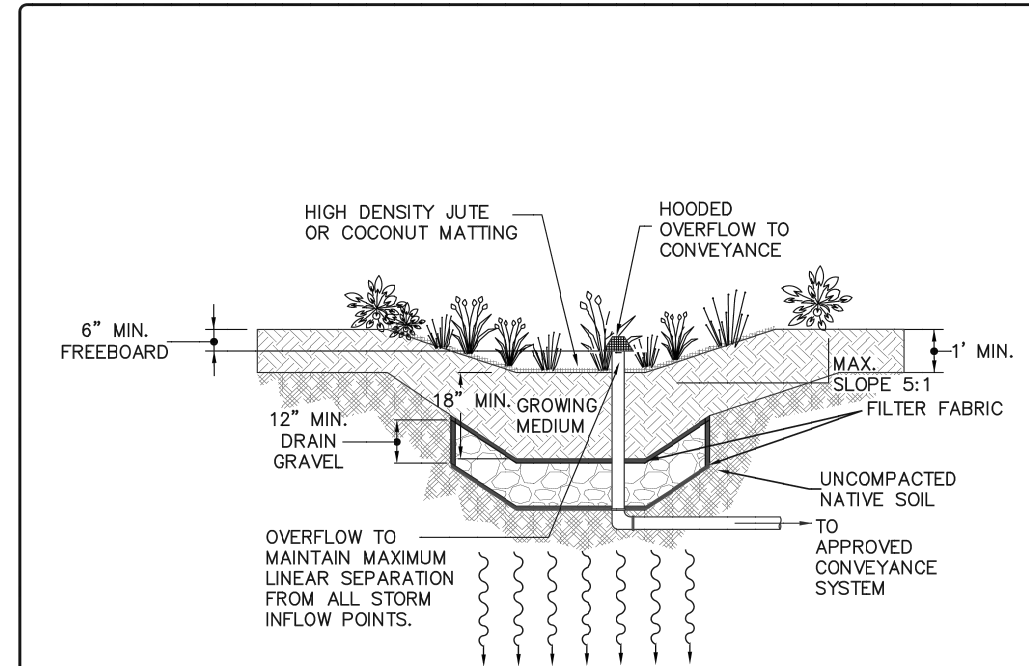
Total Impervious Area Treated (sq. ft.) MUST BE EQUAL TO (IA)



NO.	DATE	DESCRIPTION
00	03/17/23	TYPE 2 APPLICATION

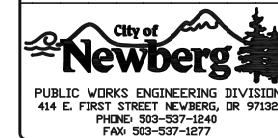
LIDA SIZING FORM

SCALE: N.T.S.
 DATE: MARCH 2014
 APPROVED BY: JAY H.
 STANDARD DRAWING: 451



NOTES:

1. PROVIDE OVERFLOW CONVEYANCE SYSTEM, OVERFLOW CONVEYANCE HEIGHT TO ALLOW 6" MAXIMUM PONDING, PIPING TO A MINIMUM OF THE PLUMBING CODE OR CONVEY THE 25 YEAR STORM.
2. FLOW DISSIPATORS SHOULD BE USED IF ENTRY SLOPE TO THE BASIN IS GREATER THAN 5:1.
3. SEPARATION BETWEEN DRAIN GRAVEL AND GROWING MEDIUM SHALL BE PERMEABLE FILTER FABRIC.
4. TREATMENT AREA SHALL HAVE HIGH DENSITY JUTE OR COCONUT MATTING OVER 18" MINIMUM OF GROWING MEDIUM OR BASE STABILIZATION METHOD AS APPROVED BY THE CITY.
5. REFER TO APPENDIX A OF THE STANDARDS DESIGN MANUAL FOR PLANTING REQUIREMENTS.
6. TOP OF BANK OF FACILITY MUST BE LOCATED 10' FROM ANY STRUCTURE AND 3' FROM ADJACENT PROPERTY LINES.



NO.	DATE	DESCRIPTION
00	03/17/23	TYPE 2 APPLICATION

RAIN GARDEN

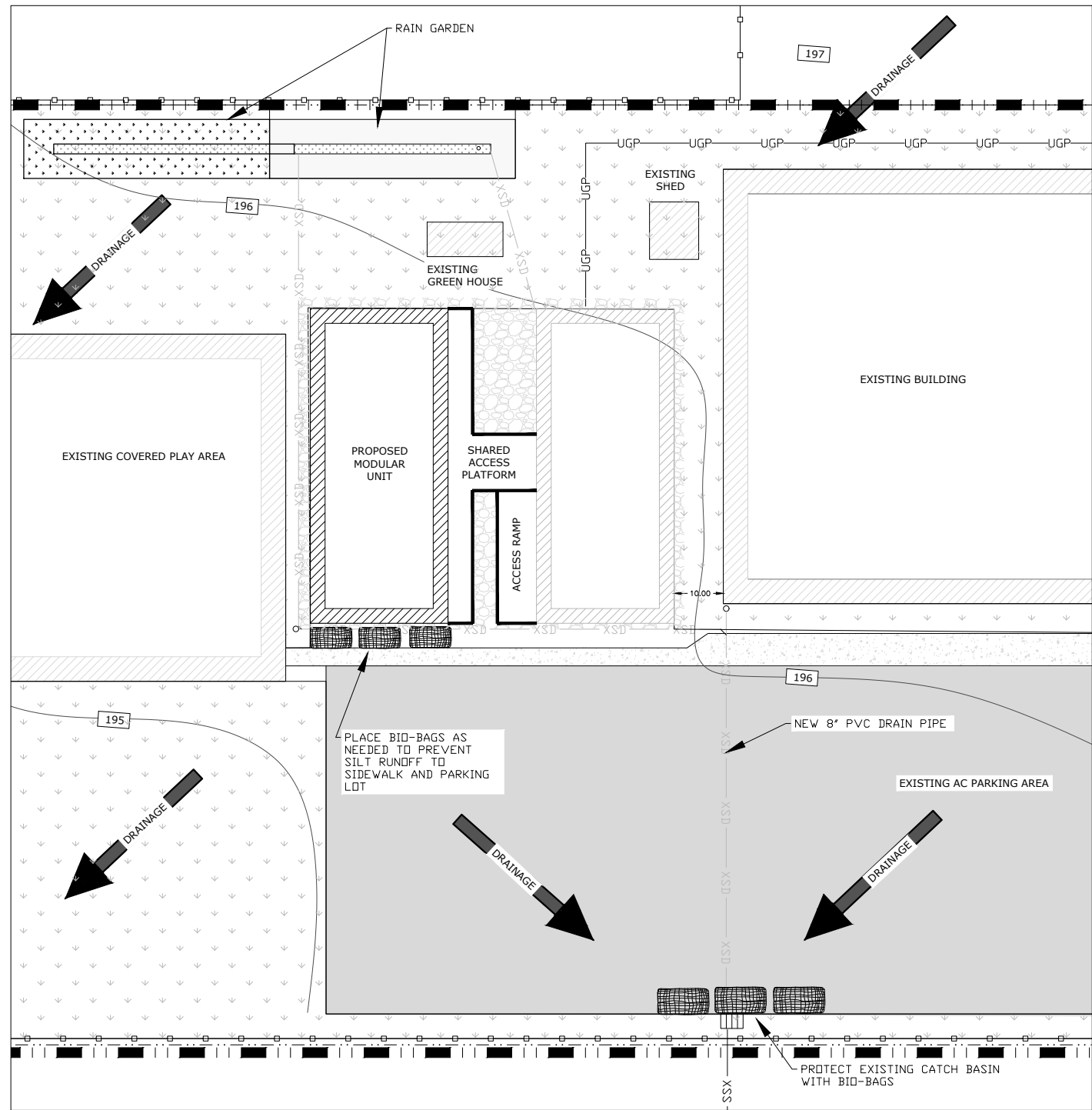
SCALE: N.T.S.
 DATE: MARCH 2014
 APPROVED BY: JAY H.
 STANDARD DRAWING: 457

PRELIMINARY

EXPIRES 12/31/23
 SHEET
02

MODULAR BUILDING ADDITION
 DETAILS

CS LEWIS ACADEMY
 1605 N. COLLEGE ST.
 NEWBERG, OR 97132



EROSION CONTROL PLAN



STANDARD EROSION AND SEDIMENT CONTROL BMP IMPLEMENTATION:

1. ALL BASE ESC MEASURES (INLET PROTECTION, PERIMETER SEDIMENT CONTROL, GRAVEL CONSTRUCTION ENTRANCES, ETC.) MUST BE IN PLACE, FUNCTIONAL, AND APPROVED IN AN INITIAL INSPECTION, PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES.
2. ALL "SEDIMENT BARRIERS (TO BE INSTALLED AFTER GRADING)" SHALL BE INSTALLED IMMEDIATELY FOLLOWING ESTABLISHMENT OF FINISHED GRADE AS SHOWN ON THESE PLANS.
3. LONG TERM SLOPE STABILIZATION MEASURES, INCLUDING MATTING, SHALL BE IN PLACE OVER EXPOSED SOILS BY OCTOBER 1.
4. INLET PROTECTION SHALL BE IN PLACE IMMEDIATELY FOLLOWING PAVING ACTIVITIES.

GRADING, STREET, AND UTILITY EROSION & SEDIMENT CONSTRUCTION NOTES:

1. SEED USED FOR TEMPORARY OR PERMANENT SEEDING SHALL BE COMPOSED OF ONE OF THE FOLLOWING MIXTURES, UNLESS OTHERWISE AUTHORIZED:
 - A. VEGETATED CORRIDOR AREAS REQUIRE NATIVE SEED MIXES. SEE RESTORATION PLAN FOR APPROPRIATE SEED MIX.
 - B. DWARF GRASS MIX (MIN. 100 LB./AC.)
DWARF PERENNIAL RYEGRASS (80% BY WEIGHT)
CREEPING RED FESCUE (20% BY WEIGHT)
 - C. STANDARD HEIGHT GRASS MIX (MIN. 100 LB./AC.)
ANNUAL RYEGRASS (40% BY WEIGHT)
TURF-TYPE FESCUE (60% BY WEIGHT)
2. SLOPE TO RECEIVE TEMPORARY OR PERMANENT SEEDING SHALL HAVE THE SURFACE ROUGHENED BY MEANS OF TRACK-WALKING OR THE USE OF OTHER APPROVED IMPLEMENTS. SURFACE ROUGHENING IMPROVES SEED BEDDING AND REDUCES RUN-OFF VELOCITY.
3. LONG TERM SLOPE STABILIZATION MEASURES SHALL INCLUDE THE ESTABLISHMENT OF PERMANENT VEGETATIVE COVER VIA SEEDING WITH APPROVED MIX AND APPLICATION RATE.
4. TEMPORARY SLOPE STABILIZATION MEASURES SHALL INCLUDE: COVERING EXPOSED SOIL WITH PLASTIC SHEETING, STRAW MULCHING, WOOD CHIPS, OR OTHER APPROVED MEASURES.
5. STOCKPILED SOIL OR STRIPPING SHALL BE PLACED IN A STABLE LOCATION AND CONFIGURATION. DURING "WET WEATHER" PERIODS, STOCKPILES SHALL BE COVERED WITH PLASTIC SHEETING OR STRAW MULCH. SEDIMENT FENCE IS REQUIRED AROUND THE PERIMETER OF THE STOCKPILE.
6. EXPOSED CUT OR FILL AREAS SHALL BE STABILIZED THROUGH THE USE OF TEMPORARY SEEDING AND MULCHING, EROSION CONTROL BLANKETS OR MATS, MID-SLOPE SEDIMENT FENCES OR WATTLES, OR OTHER APPROPRIATE MEASURES. SLOPES EXCEEDING 25% MAY REQUIRE ADDITIONAL EROSION CONTROL MEASURES.
7. AREAS SUBJECT TO WIND EROSION SHALL USE APPROPRIATE DUST CONTROL MEASURES INCLUDING THE APPLICATION OF A FINE SPRAY OF WATER, PLASTIC SHEETING, STRAW MULCHING, OR OTHER APPROVED MEASURES.
8. CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES INCLUDING, BUT NOT LIMITED TO, TIRE WASHES, STREET SWEEPING, AND VACUUMING MAY BE REQUIRED TO INSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.
9. ACTIVE INLETS TO STORM WATER SYSTEMS SHALL BE PROTECTED THROUGH THE USE OF APPROVED INLET PROTECTION MEASURES. ALL INLET PROTECTION MEASURES ARE TO BE REGULARLY INSPECTED AND MAINTAINED AS NEEDED.
10. SATURATED MATERIALS THAT ARE HAULED OFF-SITE MUST BE TRANSPORTED IN WATER-TIGHT TRUCKS TO ELIMINATE SPILLAGE OF SEDIMENT AND SEDIMENT-LADEN WATER.
11. AN AREA SHALL BE PROVIDED FOR THE WASHING OUT OF CONCRETE TRUCKS IN A LOCATION THAT DOES NOT PROVIDE RUN-OFF THAT CAN ENTER THE STORM WATER SYSTEM. IF THE CONCRETE WASH-OUT AREA CAN NOT BE CONSTRUCTED GREATER THAN 50' FROM ANY DISCHARGE POINT, SECONDARY MEASURES SUCH AS BERMS OR TEMPORARY SETTLING PITS MAY BE REQUIRED. THE WASH-OUT SHALL BE LOCATED WITHIN 6' OF TRUCK ACCESS AND BE CLEANED WHEN IT REACHES 50% OF THE CAPACITY.
12. SWEEPING FROM EXPOSED AGGREGATE CONCRETE SHALL NOT BE TRANSFERRED TO THE STORM WATER SYSTEM.
13. AVOID PAVING IN WET WEATHER WHEN PAVING CHEMICALS CAN RUN-OFF IN TO THE STORM WATER SYSTEM.
14. USED BMP'S SUCH AS CHECK-DAMS, BERMS, AND INLET PROTECTION TO PREVENT RUN-OFF FROM REACHING DISCHARGE POINTS.
15. COVER CATCH BASINS, MANHOLES, AND OTHER DISCHARGE POINTS WHEN APPLYING SEAL COAT, TACK COAT, ETC. TO PREVENT INTRODUCING THESE MATERIALS TO THE STORM WATER SYSTEM.

CS LEWIS ACADEMY
1605 N. COLLEGE ST.
NEWBERG, OR 97132

MODULAR BUILDING ADDITION
EROSION CONTROL PLAN

REVISIONS	
NO.	DESCRIPTION
00	03/17/23 TYPE 2 APPLICATION

PREPARED BY:
DANIEL DANICIC, PE
YAMHILL LAND DEVELOPMENT SERVICES
PO BOX 1042
NEWBERG, OR 97132
D.D.DANICIC@GMAIL.COM
503-476-7702

PRELIMINARY

SHEET
ESC 1

**FIRE CODE / LAND USE / BUILDING REVIEW
APPLICATION**



North Operating Center
11945 SW 70th Avenue
Tigard, OR 97223
Phone: 503-649-8577

South Operating Center
8445 SW Elligsen Rd
Wilsonville, OR 97070
Phone: 503-649-8577

REV 6-30-20

Project Information

Applicant Name: Daniel Danicic, Yamhill Land Development Services LLC

Address: PO Box 1042

Phone: 503-4767702

Email: djd.ylds@gmail.com

Site Address: 1605 N College St

City: Newberg

Map & Tax Lot #: 322118AD – 6900, 7000 and 7100

Business Name: CS Lewis Academy

Land Use/Building Jurisdiction: Newberg

Land Use/ Building Permit # N/A

Choose from: Beaverton, Tigard, Newberg, Tualatin, North Plains, West Linn, Wilsonville, Sherwood, Rivergrove, Durham, King City, Washington County, Clackamas County, Multnomah County, Yamhill County

Project Description

Placement of modular classroom unit relocated from Newberg School District, Edwards Elementary onto the CS Lewis Academy site.

Permit/Review Type (check one):

Land Use / Building Review - Service Provider Permit

Emergency Radio Responder Coverage Install/Test

LPG Tank (Greater than 2,000 gallons)

Flammable or Combustible Liquid Tank Installation (Greater than 1,000 gallons)

* Exception: Underground Storage Tanks (UST) are deferred to DEQ for regulation.

Explosives Blasting (Blasting plan is required)

Exterior Toxic, Pyrophoric or Corrosive Gas Installation (in excess of 810 cu.ft.)

Tents or Temporary Membrane Structures (in excess of 10,000 square feet)

Temporary Haunted House or similar

OLCC Cannabis Extraction License Review

Ceremonial Fire or Bonfire (For gathering, ceremony or other assembly)

For Fire Marshal's Office Use Only

TVFR Permit # 2023 - 0049

Permit Type: SPP - Newberg

Submittal Date: 4/10/23

Assigned To: DFM Ann

Due Date: NA

Fees Due: Ø

Fees Paid: Ø

Approval/Inspection Conditions
(For Fire Marshal's Office Use Only)

This section is for application approval only

[Signature] 0430
Fire Marshal or Designee

4/10/23
Date

Conditions: see attached approved FS plans.

See Attached Conditions: Yes No

Site Inspection Required: Yes No

This section used when site inspection is required

Inspection Comments:

Final TVFR Approval Signature & Emp ID _____ Date _____

TITLE PAGE

**CS LEWIS
MODULAR CLASSROOM
INSTALLATION**

1605 N COLLEGE ST
NEWBERG, OR 97132

REVISIONS

NO.	DATE	DESCRIPTION
00	03/17/23	TYPE 2 APPLICATION

PREPARED BY:
YAMHILL LAND DEVELOPMENT SERVICES LLC
PO BOX 1042 97132
NEWBERG, OR
DUB.UA@YLDSPRIVAL.COM



APPROVED PLANS

APPROVAL OF PLANS IS NOT AN APPROVAL
OR OMISSION OF RESPONSIBILITIES

Tyson O'Neil
Deputy Fire Marshal II

TVFR Permit# 2023-0049

SHEET

G1

PROJECT CONTACTS

CLIENT:
CS LEWIS ACADEMY
1605 N COLLEGE ST
NEWBERG, OR 97132

ENGINEER AND DEVELOPMENT SERVICES LLC
PO BOX 1042
NEWBERG, OR 97132
DANIEL DAMIC, PE
503-419-9772

ENGINEER'S NOTE TO CONTRACTOR:

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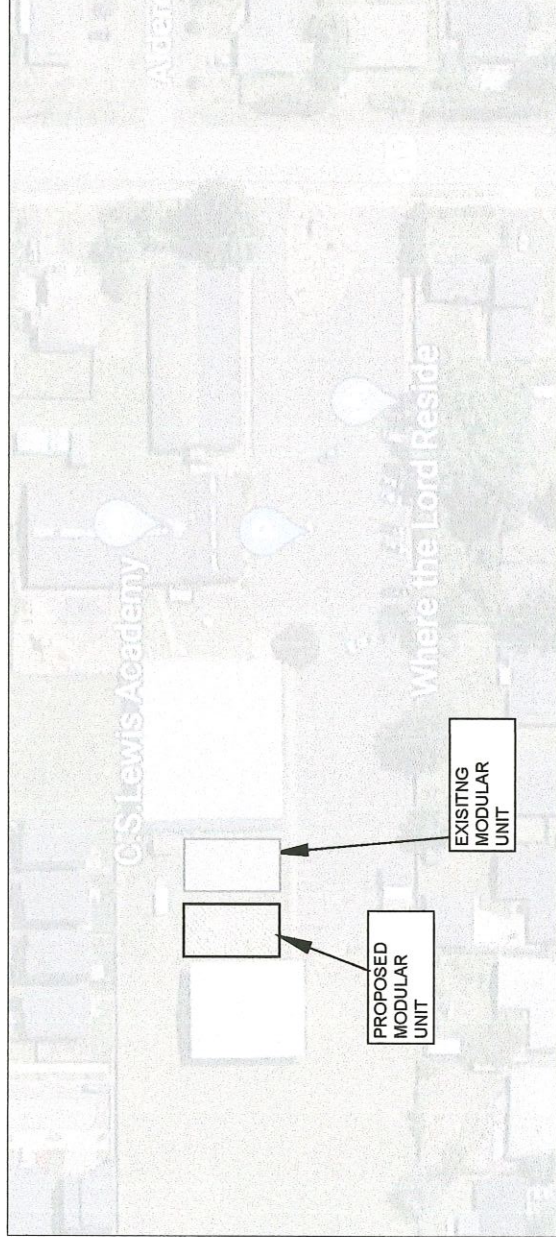
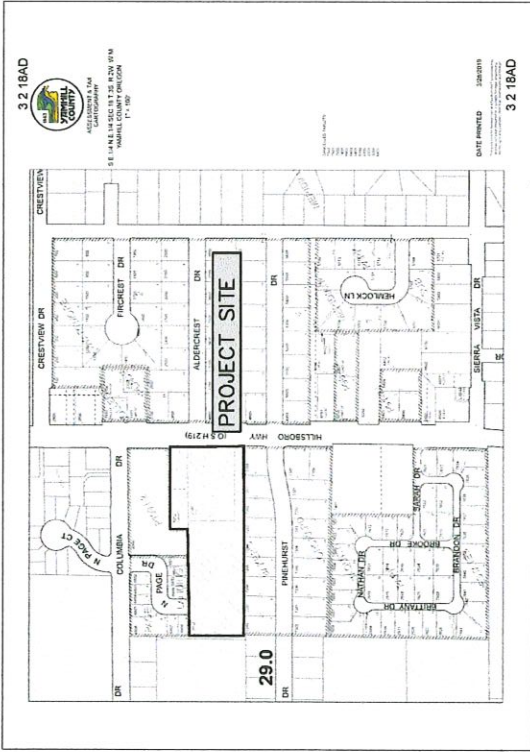
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POTENTIAL UNDERGROUND FACILITY OWNERS
Dig Safely.
Call the 1-800-ONE-Call Center
DIAL 811 or 1-800-332-2344

SHEET INDEX

- G1 TITLE PAGE
- C1 SITE PLAN
- B1 FLOOR PLAN
- FS1 FIRE SERVICE PLAN



SITE PLAN

CS LEWIS
MODULAR CLASSROOM
INSTALLATION
1605 N COLLEGE ST
NEWBERG, OR 97132

NO.	DATE	DESCRIPTION
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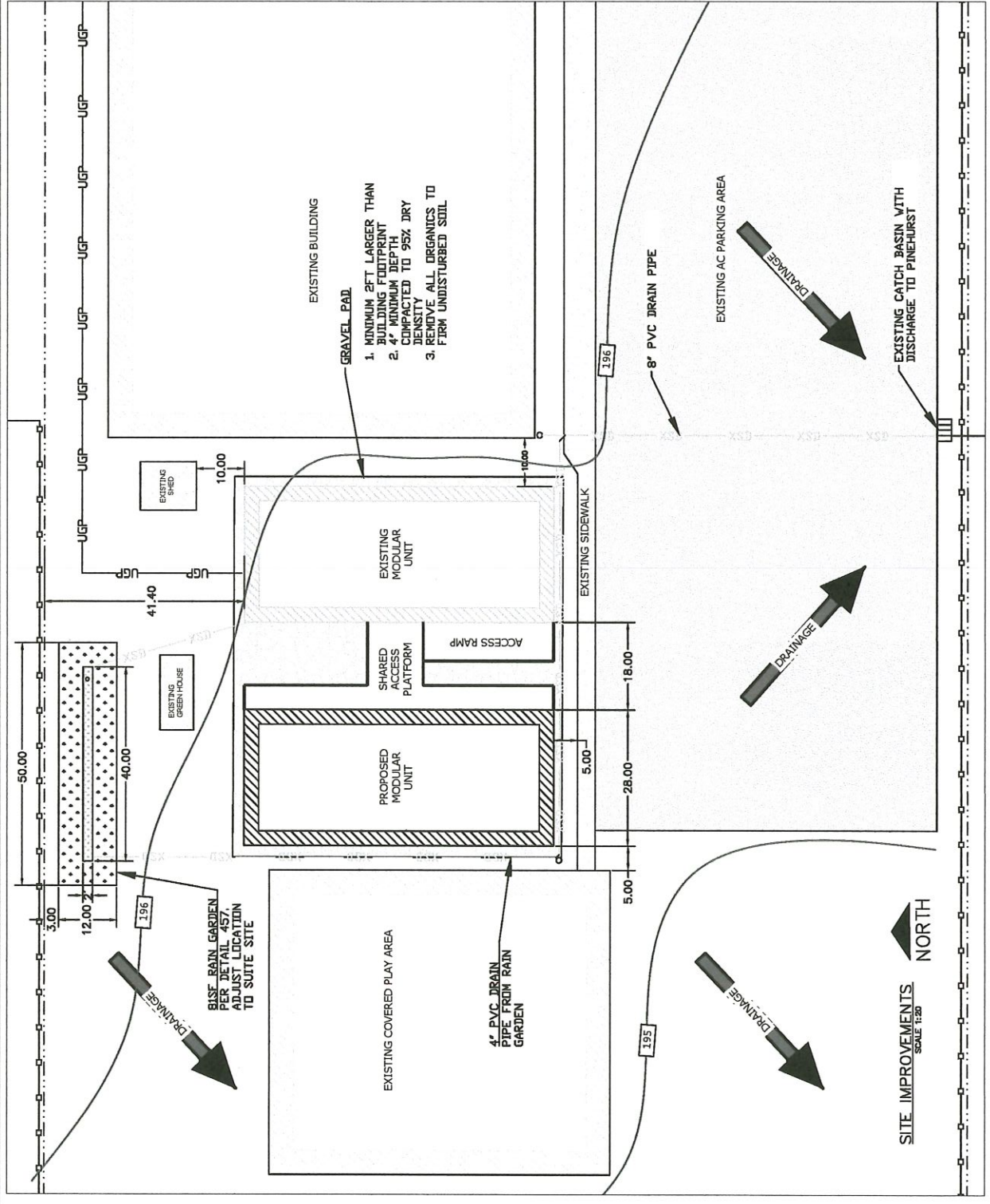
PREPARED BY
YAMHILL LAND DEVELOPMENT SERVICES LLC
1605 N COLLEGE ST
NEWBERG, OR 97132
DANIEL BRANCK
DUB@YLDSPRIMAL.COM



APPROVED PLANS
APPROVAL OF PLANS IS NOT AN APPROVAL
OR OMISSIONS OR OVERSIGHTS.
Tyson O'Neil
Deputy Fire Marshal II

TVFR Permit# 2023-0049

SHEET
C1



FLOOR PLAN

CS LEWIS
MODULAR CLASSROOM
INSTALLATION
1605 N COLLEGE ST
NEWBERG, OR 97132

NO.	DATE	DESCRIPTION
00	03/17/23	TYPE 2 APPLICATION

PREPARED BY
YAMHILL LAND DEVELOPMENT SERVICES LLC
PO BOX 1042
NEWBERG, OR 97132
503.538.2200
YDL@YDPMALL.COM

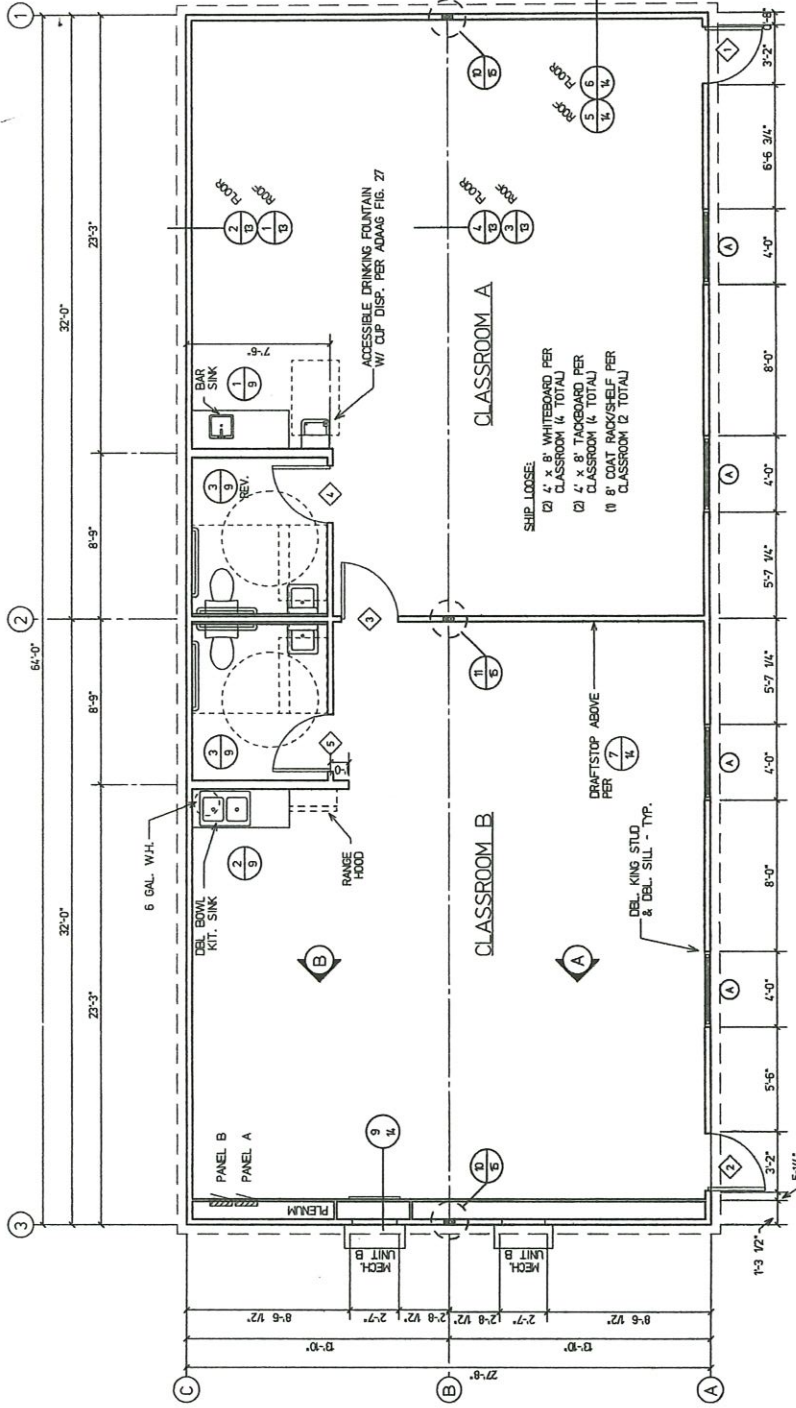


APPROVED PLANS
APPROVAL OF PLANS IS NOT AN APPROVAL
OF OMISSIONS OR OVERSIGHTS.

Debra O. Herzhall
Deputy Fire Marshal II
TVFR Permit# 2023-0049

SHEET

B1



FLOOR PLAN

FIRE SERVICE PLAN

CS LEWIS
MODULAR CLASSROOM
INSTALLATION
1605 N COLLEGE ST
NEWBERG, OR 97132

NO.	DATE	DESCRIPTION
00	03/17/23	TYPE 2 APPLICATION

PREPARED BY
YAMHILL LAND DEVELOPMENT SERVICES LLC
PO BOX 1642
NEWBERG, OR 97132
DANIEL DANICS, PE
DAN@YLDSDS.COM

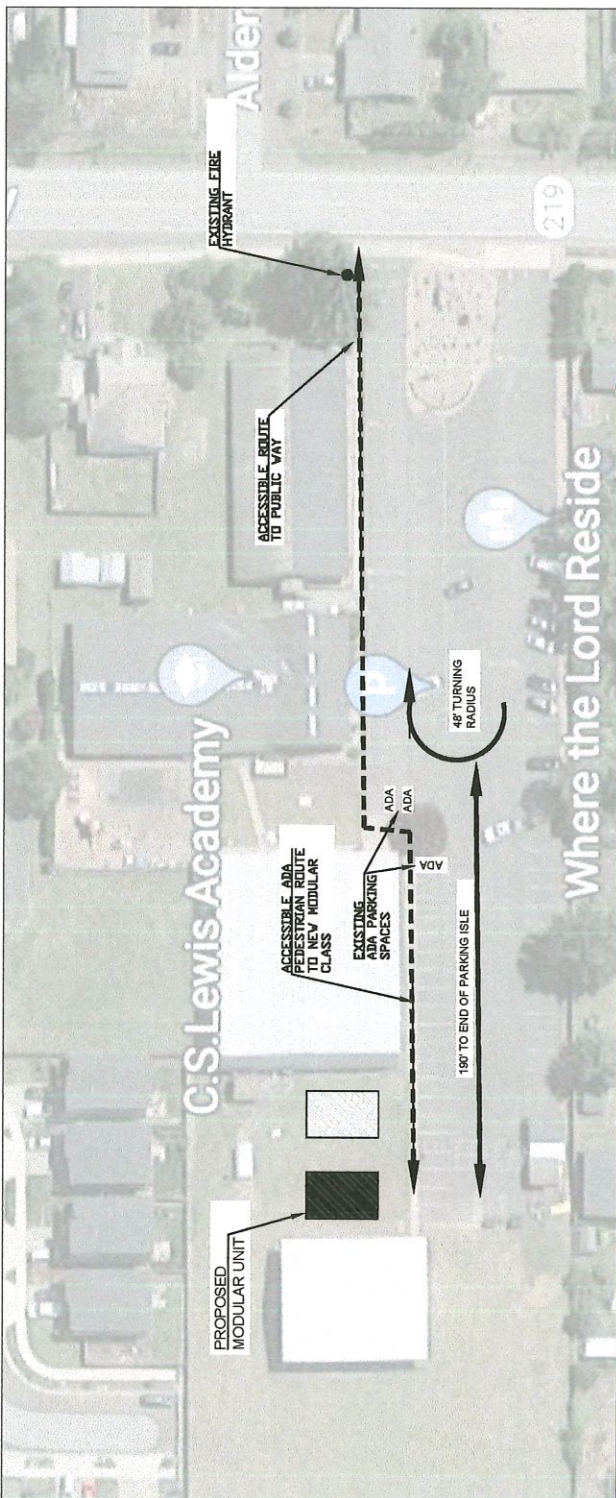


APPROVED PLANS
APPROVAL OF PLANS IS NOT AN APPROVAL
OF OMISSIONS OR OVERSIGHTS.

Jason David Gardner
Deputy Fire Marshal II
TVFR Permit# 2023-0049

SHEET

FS1



Fire Hydrant Fire Flow	
PROJECT	CS Lewis
ADDRESS	1605 N College St Newberg, OR 97132
CROSS STREET	
FLOW	
HYDRANT LOCATION	1605 N College St
STATIC	86
PITOT RESIDUAL	27
GPM	1954
GPM AT 20 PSI	7132 7% drop
DATED	07/12/21
MONITOR	
HYDRANT LOCATION	N College St & Pinehurst Dr
STATIC	86
RESIDUAL	80
DATED	07/12/21
WITNESS	
WITNESSED BY	
TITLE	
ORGANIZATION	City of Newberg
SIGNATURE	
PERFORMED BY	Jack Gardner

Attachment 2: Agency Comments



COMMUNITY DEVELOPMENT
LAND USE APPLICATION REFERRAL

Brooks Bateman

The enclosed material has been referred to you for your information and comment. Any comments you wish to make should be returned to the Community Development Department prior to: May 9, 2023
Please refer questions and comments to: Clay Downing

NOTE: Full size plans are available at the Community Development Department Office.

APPLICANT: Yamhill Land Development Services LLC
REQUEST: Placement of Modular Classroom
SITE ADDRESS: 1605 N College St
LOCATION: N/A
TAX LOT: R3218AD 07000
FILE NO: DR223-0002/ADJC23-0004
ZONE: R-1(Residential Low Density Zoning
HEARING DATE: N/A



Project Information is Attached:

- Reviewed, no conflict.
- Reviewed; recommend denial for the following reasons:
- Require additional information to review. (Please list information required)
- Meeting requested.
- Comments. (Attach additional pages as needed)

ALL ADA AND CODE REQUIREMENTS WILL NEED TO BE MET. ~~ADD SETUP~~ BROOKS BATEMAN 4-25-23

Reviewed By:

Date:

BUILDING
Organization:



COMMUNITY DEVELOPMENT LAND USE APPLICATION REFERRAL

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- Comments. (Attach additional pages as needed)

Digitally signed by Will
DNE, CN=US, OU=Will Worthey, O=City of Newberg, CN=Will
E=will.worthey@newbergoregon.gov
Reason: I am the author of this document
Location: your signing location here
Date: 2023.04.25 07:24:07-07'00"
Foxit PhantomPDF, Version: 10.1.9

4/25/23

Reviewed By:

Date:

Will Worthey CM

Organization:



COMMUNITY DEVELOPMENT
LAND USE APPLICATION REFERRAL

Doug Rux

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HEARING DATE: N/A



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- Comments. (Attach additional pages as needed)

Doug Rux

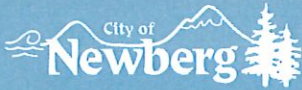
Reviewed By:

5/2/23

Date:

City of Newberg

Organization:



COMMUNITY DEVELOPMENT LAND USE APPLICATION REFERRAL

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LOCATION: N/A
TAX LOT: R3218AD 07000
FILE NO: DR223-0002/ADJC23-0004
ZONE: R-1(Residential Low Density Zoning)
HEARING DATE: N/A



Project Information is Attached:

- Reviewed, no conflict.
- Reviewed; recommend denial for the following reasons:
- Require additional information to review. (Please list information required) *City lien on Property*
- Meeting requested.
- Comments. (Attach additional pages as needed)

Clay Downing
Reviewed By:

4/28/23
Date:

Finance
Organization:

*has 3 city liens in TOTAL
Newberg Open Bible*

City of Newberg Interest In Real Property

**Internal Municipality Use Only
Not Valid for Commercial Title Search**

Payoff calculations have not been completed.

Use payoff calculator in Conduits web interface.

This document serves as constructive notice of the
City of Newberg's interest in the real property identified below.

Property Address	
Zone	
Maptaxlot Number	03S02W18AD07000

Search performed by **Barbara Davis** of **City of Newberg** on Apr 28, 2023 at 10:15:39 A.M. PDT

Tracking Number: 2231223

Access PIN: 73087



City of Newberg Interest In Real Property

LIEN 1 of 1

Account Number:	0675
Status:	Active
Principal Balance:	\$2,157.92
Interest Balance:	\$7.18
Project Description:	College Street LID - College Street LID
Amounts Due:	
Due Date:	02/23/2023
Principal and Interest Due:	\$0.00
Penalties:	\$0.00
Fees:	\$0.00
Terms:	
Payment Amount:	\$55.81
Start Date:	03/25/2016
Term:	360 Months
Billing Cycle:	Semi-Annual
Payment Type:	Amortized
Interest Rate:	1.5%
Past Due Interest Rate:	1.5%
Fee:	\$0.00





City of Newberg Interest In Real Property

**Internal Municipality Use Only
Not Valid for Commercial Title Search**

Payoff calculations have not been completed.
Use payoff calculator in Conduits web interface.

This document serves as constructive notice of the
City of Newberg's interest in the real property identified below.

Property Address	1605 N COLLEGE ST
Zone	
Maptaxlot Number	03S02W18AD07100

Search performed by **Barbara Davis** of **City of Newberg** on Apr 28, 2023 at 10:14:30 A.M. PDT

Tracking Number: 2231220

Access PIN: 33407

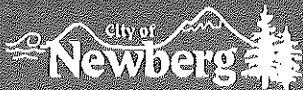


City of Newberg Interest In Real Property

LIEN 1 of 1

Account Number:	0674
Status:	Active
Principal Balance:	\$11,710.15
Interest Balance:	\$38.98
Project Description:	College Street LID - College Street LID
Amounts Due:	
Due Date:	02/23/2023
Principal and Interest Due:	\$0.00
Penalties:	\$0.00
Fees:	\$0.00
Terms:	
Payment Amount:	\$302.71
Start Date:	03/25/2016
Term:	360 Months
Billing Cycle:	Semi-Annual
Payment Type:	Amortized
Interest Rate:	1.5%
Past Due Interest Rate:	1.5%
Fee:	\$0.00





COMMUNITY DEVELOPMENT
LAND USE APPLICATION REFERRAL

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FILE NO: DR223-0002/ADJC23-0004
ZONE: R-1(Residential Low Density Zoning)
HEARING DATE: N/A



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- Meeting requested.
- Comments. (Attach additional pages as needed)



Reviewed By:

4/26/2023
Date:


Organization



COMMUNITY DEVELOPMENT LAND USE APPLICATION REFERRAL

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Please refer questions and comments to: Clay Downing

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LOCATION: N/A
TAX LOT: R3218AD 07000
FILE NO: DR223-0002/ADJC23-0004
ZONE: R-1(Residential Low Density Zoning)
HEARING DATE: N/A



Project Information is Attached:

Reviewed, no conflict.

Reviewed; recommend denial for the following reasons:

Require additional information to review. (Please list information required)

Meeting requested.

Comments. (Attach additional pages as needed)

Reviewed By:

Date:

Organization:



COMMUNITY DEVELOPMENT LAND USE APPLICATION REFERRAL

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FILE NO: DR223-0002/ADJC23-0004
ZONE: R-1(Residential Low Density Zoning)
HEARING DATE: N/A



Project Information is Attached:

Reviewed, no conflict.

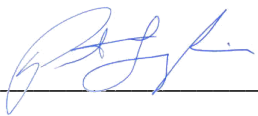
Reviewed; recommend denial for the following reasons:

Require additional information to review. (Please list information required)

Meeting requested.

Comments. (Attach additional pages as needed)

Comments on Page 42 of this
Packet



Reviewed By:

Date:

Organization:

All building run off should go to the rain garden first before being discharged. How does any of the new or existing building accomplish this?

EXTEND RAIN GARDEN TO PROVIDE AN ADDITIONAL 81SF OF TREATMENT AREA. SEE SHEET 3 FOR DETAILS

EXISTING RAIN GARDEN

IMAGE

EXISTING COVERED PLAY AREA

4" PVC DRAIN PIPE FROM RAIN GARDEN

PROPOSED MODULAR UNIT

SHARED ACCESS PLATFORM

ACCESS RAMP

EXISTING MODULAR UNIT

EXISTING SIDEWALK

EXISTING BUILDING

GRAVEL PAD

1. MINIMUM 2FT LARGER THAN BUILDING FOOTPRINT
2. 4" MINIMUM DEPTH COMPACTED TO 95% DRY DENSITY
3. REMOVE ALL ORGANICS TO FIRM UNDISTURBED SOIL

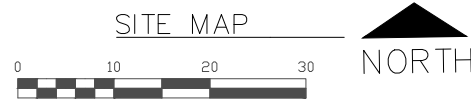
196

195

8" PVC DRAIN PIPE

EXISTING AC PARKING AREA

EXISTING CATCH BASIN WITH DISCHARGE TO PINEHURST



196

5.00

28.00

18.00

50.00

41.40

10.00

10.00

CS LEWIS ACADEMY
1605 N. COLLEGE ST.
NEWBERG, OR 97132

MODULAR BUILDING ADDITION
SITE IMPROVEMENTS

NO.	DATE	DESCRIPTION
00	03/17/23	TYPE 2 APPLICATION

PREPARED BY:
DANIEL DANICIC, PE
YAMHILL LAND DEVELOPMENT SERVICES
PO BOX 1042
NEWBERG, OR 97132
DJD.YLDS@GMAIL.COM
503-476-7702

EXPIRES 12/31/23
SHEET

C1

PRELIMINARY



COMMUNITY DEVELOPMENT LAND USE APPLICATION REFERRAL

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REQUEST: Placement of Modular Classroom
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LOCATION: N/A
TAX LOT: R3218AD 07000
FILE NO: DR223-0002/ADJC23-0004
ZONE: R-1(Residential Low Density Zoning)
HEARING DATE: N/A

RECEIVED

4/25/2023

batesf

Project Information is Attached:

- Reviewed, no conflict.
- Reviewed; recommend denial for the following reasons:
- Require additional information to review. (Please list information required)
- Meeting requested.
- Comments. (Attach additional pages as needed)

Reviewed By:

City of Newberg - Operations

Organization:

4/25/23

Date:



COMMUNITY DEVELOPMENT LAND USE APPLICATION REFERRAL

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LOCATION: N/A
TAX LOT: R3218AD 07000
FILE NO: DR223-0002/ADJC23-0004
ZONE: R-1(Residential Low Density Zoning
HEARING DATE: N/A



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- Comments. (Attach additional pages as needed)

April Catan

Digitally signed by April Catan
DN: cn=US, ou=Operations, o=City of Newberg, cn=April Catan,
E=april.catan@newbergoregon.gov
Reason: I am the author of this document
Location: your signing location here
Date: 2023.04.25 06:23:41-07'00'
Post-PhantomPDF, Version: 10.1.0

4/25/23

Reviewed By:

Date:

City of Newberg

Organization:

ENGINEERING COMMENTS

May 7, 2023

FILE NO: DR223-0002
REQUEST: CS Lewis Academy Modular Classroom Addition (second)
LOCATION: 1605 N College Street

SITE INFORMATION:

Access and Transportation: Access to the proposed development is provided from N College Street. It is classified as a minor arterial under the jurisdiction of Oregon Department of Transportation (ODOT).

Utilities:

Water: There is an 8-inch water line located on N College Street with a 2-inch service lateral serving the property. Fire flow will need to be confirmed by a fire flow test.

Wastewater: There is a 21-inch wastewater line located on N College Street.

Stormwater: There is a 12-inch storm line located in Pinehurst Drive with a 12-inch lateral serving the property.

Overhead Lines: Any new connection the property will need to be undergrounded. See NMC 15.430.010 for exception provisions.

Chapter 12.05 Street and Sidewalks

12.05.090 Permits and certificates.

A. Concurrent with the issuance of a building permit for the construction of a building for residential use or business structures or an addition to a dwelling or business structure, the value of which is \$30,000 or more except as the city engineer may require on building permits of lesser value in accordance with NMC 12.05.040, the owner, builder or contractor to whom the building permit is issued shall meet the following requirements:

1. Construct a sidewalk within the dedicated right-of-way for the full frontage in which a sidewalk in good repair does not exist. The sidewalk construction shall be completed within the building construction period or prior to issuance of an occupancy permit, whichever is the lesser.

Finding: Sidewalks improvements along the frontage have been completed previously.

This criterion is not applicable.

2. Dedicate right-of-way in accordance with the city transportation plan.

Finding: There is adequate right-of-way along the property frontage.

This criterion is not applicable.

Chapter 15.220 Site Design Review

15.220.030 Site design review requirements.

B. Type II. The following information is required to be submitted with all Type II applications for site design review:

13. Roadways and Utilities. The proposed plans shall indicate any public improvements that will be constructed as part of the project, including, but not limited to, roadway and utility improvements.

Finding: No public improvements are proposed or required.

This criterion is not applicable.

14. Traffic Study. A traffic study shall be submitted for any project that generates in excess of 40 trips per p.m. peak hour. This requirement may be waived by the director when a determination is made that a previous traffic study adequately addresses the proposal and/or when off-site and frontage improvements have already been completed which adequately mitigate any traffic impacts and/or the proposed use is not in a location which is adjacent to an intersection which is functioning at a poor level of service. A traffic study may be required by the director for projects below 40 trips per p.m. peak hour where the use is located immediately adjacent to an intersection functioning at a poor level of service. The traffic study

shall be conducted according to the City of Newberg design standards. [Ord. 2619, 5-16-05; Ord. 2451, 12-2-96. Code 2001 § 151.192.]

Finding: The Institute of Transportation Engineers (ITE), Trip Generation Manual, 10th Edition, Trip Code 536 Private School (K-12) cites the PM peak hour vehicle trip rate is 5.5 per 1,000 square feet of gross floor area. For the proposed modular addition of 1,792 square feet this equates to 9.9 pm peak hour vehicle trips.

This criterion is not applicable.

15.430.010 Underground utility installation.

Chapter 15.430 Underground Utility Installation
A. All new utility lines, including but not limited to electric, communication, natural gas, and cable television transmission lines, shall be placed underground. This does not include surface-mounted transformers, connections boxes, meter cabinets, service cabinets, temporary facilities during construction, and high-capacity electric lines operating at 50,000 volts or above.

B. Existing utility lines shall be placed underground when they are relocated, or when an addition or remodel requiring a Type II design review is proposed, or when a developed area is annexed to the city.

C. The director may make exceptions to the requirement to underground utilities based on one or more of the following criteria:

- 1. The cost of undergrounding the utility is extraordinarily expensive.**
- 2. There are physical factors that make undergrounding extraordinarily difficult.**
- 3. Existing utility facilities in the area are primarily overhead and are unlikely to be changed. [Ord. 2537, 11-6-00. Code 2001 § 151.589.]**

This criterion will be met if the aforementioned condition of approval is adhered to.
Finding: The submitted plans do not show new connections to utilities. Any new utility connections will be required to be installed underground.
Chapter 15.505 Public Improvement Standards

15.505.010 Purpose.

This chapter provides standards for public infrastructure and utilities installed with new development, consistent with the policies of the City of Newberg comprehensive plan and adopted city master plans. The standards are intended to minimize disturbance to natural features, promote energy conservation and efficiency, minimize and maintain development

impacts on surrounding properties and neighborhoods, and ensure timely completion of adequate public facilities to serve new development. [Ord. 2810 § 2 (Exhs. B, C), 12-19-16.]

15.505.020 Applicability.

The provision and utilization of public facilities and services within the City of Newberg shall apply to all land developments in accordance with this chapter. No development shall be approved unless the following improvements are provided for prior to occupancy or operation, unless future provision is assured in accordance with NMC 15.505.030(E).

Finding: A

All improvements reviewed under this application are identified in the NMC 15.505 section specific to them and are conditioned to comply with the Public Works Design and Construction Standards in those sections.

This site ~~is a~~ ***Public Works Design and Construction Standards. The design and construction of all improvements within existing and proposed rights-of-way and easements, all improvements to be maintained by the city, and all improvements for which city approval is required shall comply with the requirements of the most recently adopted Newberg public works design and construction standards.***

Finding: All improvements reviewed under this application are identified in the NMC 15.505 section specific to them and are conditioned to comply with the Public Works Design and Construction Standards in those sections.

This criterion is met.

B. Street Improvements. All projects subject to a Type II design review, partition, or subdivision approval must construct street improvements necessary to serve the development.

Finding: N College Street adjacent to the proposed development is already improved.

This criterion is met.

Finding: No new connection to the public water line is proposed. There is a hydrant at the entrance to the site. The applicant submitted a fire flow test performed by Wyatt Fire Protect Inc. These results need to be submitted with the building permit application to be reviewed by the Fire Marshall for approval.

C. Water. All developments, lots, and parcels within the City of Newberg shall be served by the municipal water system as specified in Chapter 15.15 NMC.

D. Wastewater. All developments, lots, and parcels within the City of Newberg shall be served by the municipal wastewater system as specified in Chapter 13.10 NMC.

This site is not connected to the municipal wastewater system as specified in Chapter 13.10 NMC. protection requirements.

Finding: No new connection to the public wastewater line is proposed. The site is connected to the public wastewater line currently.

This criterion is met.

E. Stormwater. All developments, lots, and parcels within the City of Newberg shall manage stormwater runoff as specified in Chapters 13.20 and 13.25 NMC.

Finding: The proposed development will create more than 500 square feet of impervious area. The applicant has submitted a LIDA sizing form to determine the size of facility needed to treat and detain the 1,792 square feet of new impervious area. The applicant plans to increase the size of an existing facility to accommodate the new run off. The existing facility drains to the 12-inch private storm line which drains to a catch basin that connects to a public storm line in Pinehurst Drive.

This criterion is met.

F. Utility Easements. Utility easements shall be provided as necessary and required by the review body to provide needed facilities for present or future development of the area.

This site is not connected to the municipal wastewater system as specified in Chapter 13.10 NMC. (ODOT). There is a public utility easement benefitting ODOT along the property frontage.

This criterion is met.

G. City Approval of Public Improvements Required. No building permit may be issued until all required public facility improvements are in place and approved by the director, or are otherwise bonded for in a manner approved by the review authority, in conformance with the provisions of this code and the Newberg Public Works Design and Construction Standards. [Ord. 2810 § 2 (Exhs. B, C), 12-19-16.]

This site is not connected to the municipal wastewater system as specified in Chapter 13.10 NMC. Any required public improvement permits for this project must be submitted, approved and issued prior to building permits being issued.

15.505.030 Street standards.

A. Purpose. The purpose of this section is to:

This criterion will be met if the aforementioned condition of approval is adhered to.

1. Provide for safe, efficient, and convenient multi-modal transportation within the City of Newberg.

2. Provide adequate access to all proposed and anticipated developments in the City of Newberg. For purposes of this section, “adequate access” means direct routes of travel between destinations; such destinations may include residential neighborhoods, parks, schools, shopping areas, and employment centers.

3. Provide adequate area in all public rights-of-way for sidewalks, wastewater and water lines, stormwater facilities, natural gas lines, power lines, and other utilities commonly and appropriately placed in such rights-of-way. For purposes of this section, “adequate area” means space sufficient to provide all required public services to standards defined in this code and in the Newberg public works design and construction standards.

B. Applicability. The provisions of this section apply to:

1. The creation, dedication, and/or construction of all public streets, bike facilities, or pedestrian facilities in all subdivisions, partitions, or other developments in the City of Newberg.

2. The extension or widening of existing public street rights-of-way, easements, or street improvements including those which may be proposed by an individual or the city, or which may be required by the city in association with other development approvals.

3. The construction or modification of any utilities, pedestrian facilities, or bike facilities in public rights-of-way or easements.

4. The designation of planter strips. Street trees are required subject to Chapter 15.420 NMC.

5. Developments outside the city that tie into or take access from city streets.

C. Layout of Streets, Alleys, Bikeways, and Walkways. Streets, alleys, bikeways, and walkways shall be laid out and constructed as shown in the Newberg transportation system plan. In areas where the transportation system plan or future street plans do not show specific transportation improvements, roads and streets shall be laid out so as to conform to previously approved subdivisions, partitions, and other developments for adjoining properties, unless it is found in the public interest to modify these patterns. Transportation improvements shall conform to the standards within the Newberg Municipal Code, the Newberg public works design and construction standards, the Newberg transportation system plan, and other adopted city plans.

D. Construction of New Streets. Where new streets are necessary to serve a new development, subdivision, or partition, right-of-way dedication and full street improvements shall be required. Three-quarter streets may be approved in lieu of full street improvements when the city finds it to be practical to require the completion of the other one-quarter street improvement when the adjoining property is developed; in such cases, three-quarter street improvements may be allowed by the city only where all of the following criteria are met:

- 1. The land abutting the opposite side of the new street is undeveloped and not part of the new development; and***
- 2. The adjoining land abutting the opposite side of the street is within the city limits and the urban growth boundary.***

Finding: N College Street is improved adjacent to the property. The requirements of A, B, C, and D are met. This criterion is met.

E. Improvements to Existing Streets.

- 1. All projects subject to partition, subdivision, or Type II design review approval shall dedicate right-of-way sufficient to improve the street to the width specified in subsection (G) of this section.***

Finding: The right-of-way adjacent to the property along N College Street is sufficient.

This criterion is met.

- 2. All projects subject to partition, subdivision, or Type II design review approval must construct a minimum of a three-quarter street improvement to all existing streets adjacent to, within, or necessary to serve the development. The director may waive or modify this requirement where the applicant demonstrates that the condition of existing streets to serve the development meets city standards and is in satisfactory condition to handle the projected traffic loads from the development. Where a development has frontage on both sides of an existing street, full street improvements are required.***

Finding: N College Street adjacent to the property is improved.

This criterion is not applicable.

- 3. In lieu of the street improvement requirements outlined in NMC 15.505.040(B), the review authority may elect to accept from the applicant monies to be placed in a fund dedicated to the future reconstruction of the subject street(s). The amount of money deposited with the city shall be 100 percent of the estimated cost of the required street improvements (including any associated utility improvements), and 10 percent of the***

estimated cost for inflation. Cost estimates used for this purpose shall be based on preliminary design of the constructed street provided by the applicant’s engineer and shall be approved by the director.

Finding: No street improvements are proposed or required. Therefore, no monies are required in lieu of street improvements.

This criterion is not applicable.

F. Improvements Relating to Impacts. *Improvements required as a condition of development approval shall be roughly proportional to the impact of the development on public facilities and services. The review body must make findings in the development approval that indicate how the required improvements are roughly proportional to the impact. Development may not occur until required transportation facilities are in place or guaranteed, in conformance with the provisions of this code. If required transportation facilities cannot be put in place or be guaranteed, then the review body shall deny the requested land use application.*

Finding: No street improvements are proposed or required.

This criterion is not applicable.

G. Street Width and Design Standards.

1. Design Standards. *All streets shall conform with the standards contained in Table 15.505.030(G). Where a range of values is listed, the director shall determine the width based on a consideration of the total street section width needed, existing street widths, and existing development patterns. Preference shall be given to the higher value. Where values may be modified by the director, the overall width shall be determined using the standards under subsections (G)(2) through (10) of this section.*

Table 15.505.030(G) Street Design Standards

Type of Street	Right-of-Way Width	Curb-to-Curb Pavement Width	Motor Vehicle Travel Lanes	Median Type	Striped Bike Lane (Both Sides)	On-Street Parking
<i>Arterial Streets</i>						
<i>Expressway**</i>	<i>ODOT</i>	<i>ODOT</i>	<i>ODOT</i>	<i>ODOT</i>	<i>ODOT</i>	<i>ODOT</i>
<i>Major arterial</i>	<i>95 – 100 feet</i>	<i>74 feet</i>	<i>4 lanes</i>	<i>TWLTL or median*</i>	<i>Yes</i>	<i>No*</i>
<i>Minor arterial</i>	<i>69 – 80 feet</i>	<i>48 feet</i>	<i>2 lanes</i>	<i>TWLTL or median*</i>	<i>Yes</i>	<i>No*</i>
<i>Collectors</i>						
<i>Major</i>	<i>57 – 80 feet</i>	<i>36 feet</i>	<i>2 lanes</i>	<i>None*</i>	<i>Yes</i>	<i>No*</i>

<i>Type of Street</i>	<i>Right-of-Way Width</i>	<i>Curb-to-Curb Pavement Width</i>	<i>Motor Vehicle Travel Lanes</i>	<i>Median Type</i>	<i>Striped Bike Lane (Both Sides)</i>	<i>On-Street Parking</i>
<i>Minor</i>	<i>61 – 65 feet</i>	<i>40 feet</i>	<i>2 lanes</i>	<i>None*</i>	<i>Yes*</i>	<i>Yes*</i>
<i>Local Streets</i>						
<i>Local residential</i>	<i>54 – 60 feet</i>	<i>32 feet</i>	<i>2 lanes</i>	<i>None</i>	<i>No</i>	<i>Yes</i>
<i>Limited residential, parking both sides</i>	<i>44 – 50 feet</i>	<i>28 feet</i>	<i>2 lanes</i>	<i>None</i>	<i>No</i>	<i>Yes</i>
<i>Limited residential, parking one side</i>	<i>40 – 46 feet</i>	<i>26 feet</i>	<i>2 lanes</i>	<i>None</i>	<i>No</i>	<i>One side</i>
<i>Local commercial/ industrial</i>	<i>55 – 65 feet</i>	<i>34 feet</i>	<i>2 lanes</i>	<i>None*</i>	<i>No*</i>	<i>Yes*</i>

* *May be modified with approval of the director. Modification will change overall curb-to-curb and right-of-way width. Where a center turn lane is not required, a landscaped median shall be provided instead, with turning pockets as necessary to preserve roadway functions.*

** *All standards shall be per ODOT expressway standards.*

Finding: No street improvements are proposed or required.

This criterion is not applicable.

2. Motor Vehicle Travel Lanes. Collector and arterial streets shall have a minimum width of 12 feet.

a. Exception.

i. Minimum lane width of 11 feet along S River Street from E First Street to E Fourteenth Street.

Finding: No new collector or arterial streets are proposed.

This criterion is not applicable.

3. Bike Lanes. Striped bike lanes shall be a minimum of six feet wide. Bike lanes shall be provided where shown in the Newberg transportation system plan.

a. Exception.

i. Minimum striped bike lane width of six feet with a one-foot wide buffer along S River Street from E First Street to the bypass.

Finding: No bike lanes are proposed or required.

This criterion is not applicable.

4. Parking Lanes. Where on-street parking is allowed on collector and arterial streets, the parking lane shall be a minimum of eight feet wide.

a. Exception.

i. Minimum parking lane width of seven feet along S River Street from the bypass to E Fourteenth Street.

Finding: There are no collector or arterial streets proposed.

This criterion is not applicable.

5. Center Turn Lanes. Where a center turn lane is provided, it shall be a minimum of 12 feet wide.

Finding: No center turn lanes are proposed.

This criterion is not applicable.

6. Limited Residential Streets. Limited residential streets shall be allowed only at the discretion of the review authority, and only in consideration of the following factors:

a. The requirements of the fire chief shall be followed.

b. The estimated traffic volume on the street is low, and in no case more than 600 average daily trips.

c. Use for through streets or looped streets is preferred over cul-de-sac streets.

d. Use for short blocks (under 400 feet) is preferred over longer blocks.

e. The total number of residences or other uses accessing the street in that block is small, and in no case more than 30 residences.

f. On-street parking usage is limited, such as by providing ample off-street parking, or by staggering driveways so there are few areas where parking is allowable on both sides.

Finding: Limited residential streets are not proposed or required.

This criterion is not applicable.

7. Sidewalks. Sidewalks shall be provided on both sides of all public streets. Minimum width is five feet.

a. Exception.

i. Twelve-foot-wide sidewalks, inclusive of the curb, with tree wells along S River Street from the bypass to E Fourteenth Street.

ii. Twelve-foot-wide shared-use path and four-foot buffer, inclusive of the curb, with tree wells along the east side of S River Street from the bypass to E Fourteenth Street.

Finding: Sidewalks on N College Street are improved.

This criterion is met.

8. Planter Strips. Except where infeasible, a planter strip shall be provided between the sidewalk and the curb line, with a minimum width of five feet. This strip shall be landscaped in accordance with the standards in NMC 15.420.020. Curb-side sidewalks may be allowed on limited residential streets. Where curb-side sidewalks are allowed, the following shall be provided:

a. Additional reinforcement is done to the sidewalk section at corners.

b. Sidewalk width is six feet.

Finding: The existing sidewalks on N College Street are curb tight. No planter strips were included in the existing street improvements completed previously.

This criterion is not applicable.

9. Slope Easements. Slope easements shall be provided adjacent to the street where required to maintain the stability of the street.

Finding: The applicant is not proposing a slope easement, and none is required.

This criterion is not applicable.

10. Intersections and Street Design. The street design standards in the Newberg public works design and construction standards shall apply to all public streets, alleys, bike facilities, and sidewalks in the city.

Finding: The applicant is not proposing street improvements, and none are required.

This criterion is not applicable.

11. The planning commission may approve modifications to street standards for the purpose of ingress or egress to a minimum of three and a maximum of six lots through a conditional use permit.

Finding: The applicant is not proposing modifications to street standards for the purpose of ingress or egress.

This criterion is not applicable.

H. Modification of Street Right-of-Way and Improvement Width. The director, pursuant to the Type II review procedures of Chapter 15.220 NMC, may allow modification to the public street standards of subsection (G) of this section, when the criteria in both subsections (H)(1) and (2) of this section are satisfied:

1. The modification is necessary to provide design flexibility in instances where:

a. Unusual topographic conditions require a reduced width or grade separation of improved surfaces; or

b. Lot shape or configuration precludes accessing a proposed development with a street which meets the full standards of this section; or

c. A modification is necessary to preserve trees or other natural features determined by the city to be significant to the aesthetic character of the area; or

d. A planned unit development is proposed and the modification of street standards is necessary to provide greater privacy or aesthetic quality to the development.

2. Modification of the standards of this section shall only be approved if the director finds that the specific design proposed provides adequate vehicular access based on anticipated traffic volumes.

Finding: The applicant has not proposed modifications to these street standards.

This criterion is not applicable.

I. Temporary Turnarounds. Where a street will be extended as part of a future phase of a development, or as part of development of an abutting property, the street may be terminated with a temporary turnaround in lieu of a standard street connection or circular cul-de-sac bulb. The director and fire chief shall approve the temporary turnaround. It shall have an all-weather surface, and may include a hammerhead-type turnaround meeting fire apparatus access road standards, a paved or graveled circular turnaround, or a paved or graveled temporary access road. For streets extending less than 150 feet and/or with no significant access, the director may approve the street without a temporary turnaround. Easements or right-of-way may be required as necessary to preserve access to the turnaround.

Finding: The applicant is not proposing a temporary turnaround.

This criterion is not applicable.

J. Topography. The layout of streets shall give suitable recognition to surrounding topographical conditions in accordance with the purpose of this code.

Finding: The applicant is not proposing street improvements.

This criterion is not applicable.

K. Future Extension of Streets. All new streets required for a subdivision, partition, or a project requiring site design review shall be constructed to be “to and through”: through the development and to the edges of the project site to serve adjacent properties for future development.

Finding: There are no future street extensions proposed as part of this project.

This criterion is not applicable.

L. Cul-de-Sacs.

1. Cul-de-sacs shall only be permitted when one or more of the circumstances listed in this section exist. When cul-de-sacs are justified, public walkway connections shall be provided wherever practical to connect with another street, walkway, school, or similar destination.

a. Physical or topographic conditions make a street connection impracticable. These conditions include but are not limited to controlled access streets, railroads, steep slopes, wetlands, or water bodies where a connection could not be reasonably made.

b. Buildings or other existing development on adjacent lands physically preclude a connection now or in the future, considering the potential for redevelopment.

c. Where streets or accessways would violate provisions of leases, easements, or similar restrictions.

d. Where the streets or accessways abut the urban growth boundary and rural resource land in farm or forest use, except where the adjoining land is designated as an urban reserve area.

2. Cul-de-sacs shall be no more than 400 feet long (measured from the centerline of the intersection to the radius point of the bulb).

3. Cul-de-sacs shall not serve more than 18 single-family dwellings.

Each cul-de-sac shall have a circular end with a minimum diameter of 96 feet, curb-to-curb, within a 109-foot minimum diameter right-of-way. For residential uses, a 35-foot radius may be allowed if the street has no parking, a mountable curb, curbside sidewalks, and sprinkler systems in every building along the street.

Finding: The applicant is not proposing a cul-de-sac.

This criterion is not applicable.

M. Street Names and Street Signs. Streets that are in alignment with existing named streets shall bear the names of such existing streets. Names for new streets not in alignment with existing streets are subject to approval by the director and the fire chief and shall not unnecessarily duplicate or resemble the name of any existing or platted street in the city. It shall be the responsibility of the land divider to provide street signs.

Finding: The applicant is not naming streets.

This criterion is not applicable.

N. Platting Standards for Alleys.

- 1. An alley may be required to be dedicated and constructed to provide adequate access for a development, as deemed necessary by the director.***
- 2. The right-of-way width and paving design for alleys shall be not less than 20 feet wide. Slope easements shall be dedicated in accordance with specifications adopted by the city council under NMC 15.505.010 et seq.***
- 3. Where two alleys intersect, 10-foot corner cut-offs shall be provided.***
- 4. Unless otherwise approved by the city engineer where topographical conditions will not reasonably permit, grades shall not exceed 12 percent on alleys, and centerline radii on curves shall be not less than 100 feet.***
- 5. All provisions and requirements with respect to streets identified in this code shall apply to alleys the same in all respects as if the word “street” or “streets” therein appeared as the word “alley” or “alleys” respectively.***

Finding: The applicant is not proposing alleys.

This criterion is not applicable.

O. Platting Standards for Blocks.

- 1. Purpose. Streets and walkways can provide convenient travel within a neighborhood and can serve to connect people and land uses. Large, uninterrupted blocks can serve as a barrier to travel, especially walking and biking. Large blocks also can divide rather than unite neighborhoods. To promote connected neighborhoods and to shorten travel distances, the following minimum standards for block lengths are established.***

2. Maximum Block Length and Perimeter. *The maximum length and perimeters of blocks in the zones listed below shall be according to the following table. The review body for a subdivision, partition, conditional use permit, or a Type II design review may require installation of streets or walkways as necessary to meet the standards below.*

<i>Zone(s)</i>	<i>Maximum Block Length</i>	<i>Maximum Block Perimeter</i>
<i>R-1</i>	<i>800 feet</i>	<i>2,000 feet</i>
<i>R-2, R-3, RP, I</i>	<i>1,200 feet</i>	<i>3,000 feet</i>

3. Exceptions.

a. If a public walkway is installed mid-block, the maximum block length and perimeter may be increased by 25 percent.

b. Where a proposed street divides a block, one of the resulting blocks may exceed the maximum block length and perimeter standards provided the average block length and perimeter of the two resulting blocks do not exceed these standards.

c. Blocks in excess of the above standards are allowed where access controlled streets, street access spacing standards, railroads, steep slopes, wetlands, water bodies, preexisting development, ownership patterns or similar circumstances restrict street and walkway location and design. In these cases, block length and perimeter shall be as small as practical. Where a street cannot be provided because of these circumstances but a public walkway is still feasible, a public walkway shall be provided.

d. Institutional campuses located in an R-1 zone may apply the standards for the institutional zone.

e. Where a block is in more than one zone, the standards of the majority of land in the proposed block shall apply.

f. Where a local street plan, concept master site development plan, or specific plan has been approved for an area, the block standards shall follow those approved in the plan. In approving such a plan, the review body shall follow the block standards listed above to the extent appropriate for the plan area.

Finding: The applicant is not proposing blocks.

This criterion is not applicable.

4. Public Pedestrian Walkways and Bicycle Access. *The approval authority in approving a land use application with conditions may require a developer to provide an access way*

where the creation of a street consistent with street spacing standards is infeasible and the creation of a cul-de-sac or dead-end street is unavoidable. A public walkway provides a connection through a block that is longer than established standards or connects the end of the street to another right-of-way or a public access easement. A public walkway shall be contained within a public right-of-way or public access easement, as required by the city. A public walkway shall be a minimum of 10 feet wide and shall provide a minimum six-foot-wide paved surface or other all-weather surface approved by the city (see subsection (S) of this section for public walkway standards).

Design features should be considered that allow access to emergency vehicles but that restrict access to non-emergency motorized vehicles.

Finding: The applicant is not proposing a public walkway and it is not required.

This criterion is not applicable.

P. Private Streets. New private streets, as defined in NMC 15.05.030, shall not be created, except as allowed by NMC 15.240.020(L)(2).

Finding: The applicant is not proposing private streets.

This criterion is not applicable.

Q. Traffic Calming.

1. The following roadway design features may be required in new street construction where traffic calming needs are anticipated:

- a. Serpentine alignment.*
- b. Curb extensions.*
- c. Traffic diverters/circles.*
- d. Raised medians and landscaping.*
- e. Other methods shown effective through engineering studies.*

2. Traffic-calming measures such as speed humps should be applied to mitigate traffic operations and/or safety problems on existing streets. They should not be applied with new street constructions.

Finding: The applicant is not proposing traffic calming, and none is required.

This criterion is not applicable.

R. Vehicular Access Standards.

1. Purpose. *The purpose of these standards is to manage vehicle access to maintain traffic flow, safety, roadway capacity, and efficiency. They help to maintain an adequate level of service consistent with the functional classification of the street. Major roadways, including arterials and collectors, serve as the primary system for moving people and goods within and through the city. Access is limited and managed on these roads to promote efficient through movement. Local streets and alleys provide access to individual properties. Access is managed on these roads to maintain safe maneuvering of vehicles in and out of properties and to allow safe through movements. If vehicular access and circulation are not properly designed, these roadways will be unable to accommodate the needs of development and serve their transportation function.*

2. Access Spacing Standards. *Public street intersection and driveway spacing shall follow the standards in Table 15.505.R below. The Oregon Department of Transportation (ODOT) has jurisdiction of some roadways within the Newberg city limits, and ODOT access standards will apply on those roadways.*

Table 15.505.R. Access Spacing Standards

<i>Roadway Functional Classification</i>	<i>Area¹</i>	<i>Minimum Public Street Intersection Spacing (Feet)²</i>	<i>Driveway Setback from Intersecting Street³</i>
<i>Expressway</i>	<i>All</i>	<i>Refer to ODOT Access Spacing Standards</i>	<i>NA</i>
<i>Major arterial</i>	<i>Urban CBD</i>	<i>Refer to ODOT Access Spacing Standards</i>	
<i>Minor arterial</i>	<i>Urban CBD</i>	<i>500 200</i>	<i>150 100</i>
<i>Major collector</i>	<i>All</i>	<i>400</i>	<i>150</i>
<i>Minor collector</i>	<i>All</i>	<i>300</i>	<i>100</i>

¹ "Urban" refers to intersections inside the city urban growth boundary outside the central business district (C-3 zone).

"CBD" refers to intersections within the central business district (C-3 zone).

"All" refers to all intersections within the Newberg urban growth boundary.

² Measured centerline to centerline.

³ The setback is based on the higher classification of the intersecting streets. Measured from the curb line of the intersecting street to the beginning of the driveway, excluding flares. If the driveway setback listed above would preclude a lot from having at least one driveway, including shared driveways or driveways on adjoining streets, one driveway is allowed as far from the intersection as possible.

Finding: The applicant is not proposing a new access.

This criterion is not applicable.

3. Properties with Multiple Frontages. *Where a property has frontage on more than one street, access shall be limited to the street with the lesser classification.*

a. For a duplex, triplex or quadplex dwelling or a cottage cluster project with frontage on two local streets, access may be permitted on both streets.

Finding: The property does not have multiple frontages. Access will remain as it is currently.

This criterion is not applicable.

4. Driveways. *More than one driveway is permitted on a lot accessed from either a minor collector or local street as long as there is at least 40 feet of lot frontage separating each driveway approach. More than one driveway is permitted on a lot accessed from a major collector as long as there is at least 100 feet of lot frontage separating each driveway approach.*

a. For a duplex, triplex or quadplex dwelling or a cottage cluster project, more than one driveway is permitted on a lot accessed from either a minor collector or local street as long as there is at least 22 feet of lot frontage separating each driveway approach.

Finding: This property has two driveways with more than 40 feet between them.

This criterion is met.

5. Alley Access. *Where a property has frontage on an alley and the only other frontages are on collector or arterial streets, access shall be taken from the alley only. The review body may allow creation of an alley for access to lots that do not otherwise have frontage on a public street provided all of the following are met:*

a. The review body finds that creating a public street frontage is not feasible.

b. The alley access is for no more than six dwellings and no more than six lots.

c. The alley has through access to streets on both ends.

d. One additional parking space over those otherwise required is provided for each dwelling. Where feasible, this shall be provided as a public use parking space adjacent to the alley.

Finding: The property proposed for development does not have alley access.

This criterion is not applicable.

6. Closure of Existing Accesses. Existing accesses that are not used as part of development or redevelopment of a property shall be closed and replaced with curbing, sidewalks, and landscaping, as appropriate.

Finding: The existing access is currently used and not proposed for closure.

This criterion is not applicable.

7. Shared Driveways.

a. The number of driveways onto arterial streets shall be minimized by the use of shared driveways with adjoining lots where feasible. The city shall require shared driveways as a condition of land division or site design review, as applicable, for traffic safety and access management purposes. Where there is an abutting developable property, a shared driveway shall be provided as appropriate. When shared driveways are required, they shall be stubbed to adjacent developable parcels to indicate future extension. "Stub" means that a driveway temporarily ends at the property line, but may be accessed or extended in the future as the adjacent parcel develops. "Developable" means that a parcel is either vacant or it is likely to receive additional development (i.e., due to infill or redevelopment potential).

b. Access easements (i.e., for the benefit of affected properties) and maintenance agreements shall be recorded for all shared driveways, including pathways, at the time of final plat approval or as a condition of site development approval.

c. No more than four lots may access one shared driveway, with the exception of cottage dwellings on individual lots that are part of a cottage cluster.

d. Shared driveways shall be posted as no parking fire lanes where required by the fire marshal.

e. Where three or more lots share one driveway, one additional parking space over those otherwise required shall be provided for each dwelling. Where feasible, this shall be provided as a common use parking space adjacent to the driveway. However, duplex, triplex, quadplex, townhouse and cottage dwellings with shared driveways shall be exempt from this standard.

Finding: There is no shared driveway on the property proposed for development.

This criterion is not applicable.

8. Frontage Streets and Alleys. The review body for a partition, subdivision, or design review may require construction of a frontage street to provide access to properties fronting an arterial or collector street.

Finding: No frontage street is proposed or required.

This criterion is not applicable.

9. ODOT or Yamhill County Right-of-Way. *Where a property abuts an ODOT or Yamhill County right-of-way, the applicant for any development project shall obtain an access permit from ODOT or Yamhill County.*

Finding: The existing access is already permitted.

This criterion is met.

10. Exceptions. *The director may allow exceptions to the access standards above in any of the following circumstances:*

a. Where existing and planned future development patterns or physical constraints, such as topography, parcel configuration, and similar conditions, prevent access in accordance with the above standards.

b. Where the proposal is to relocate an existing access for existing development, where the relocated access is closer to conformance with the standards above and does not increase the type or volume of access.

c. Where the proposed access results in safer access, less congestion, a better level of service, and more functional circulation, both on street and on site, than access otherwise allowed under these standards.

Finding: The applicant is not requesting an exception.

This criterion is not applicable.

11. Where an exception is approved, *the access shall be as safe and functional as practical in the particular circumstance. The director may require that the applicant submit a traffic study by a registered engineer to show the proposed access meets these criteria.*

Finding: The applicant is not requesting an exception.

This criterion is not applicable.

S. Public Walkways.

1. Projects subject to Type II design review, partition, or subdivision approval may be required to provide public walkways where necessary for public safety and convenience, or

where necessary to meet the standards of this code. Public walkways are meant to connect cul-de-sacs to adjacent areas, to pass through oddly shaped or unusually long blocks, to provide for networks of public paths according to adopted plans, or to provide access to schools, parks or other community destinations or public areas. Where practical, public walkway easements and locations may also be used to accommodate public utilities.

2. Public walkways shall be located within a public access easement that is a minimum of 15 feet in width.

3. A walk strip, not less than 10 feet in width, shall be paved in the center of all public walkway easements. Such paving shall conform to specifications in the Newberg public works design and construction standards.

4. Public walkways shall be designed to meet the Americans with Disabilities Act requirements.

5. Public walkways connecting one right-of-way to another shall be designed to provide as short and straight of a route as practical.

6. The developer of the public walkway may be required to provide a homeowners' association or similar entity to maintain the public walkway and associated improvements.

7. Lighting may be required for public walkways in excess of 250 feet in length.

8. The review body may modify these requirements where it finds that topographic, preexisting development, or similar constraints exist.

Finding: The applicant is not proposing a public walkway, and none is required.

This criterion is not applicable.

T. Street Trees. *Street trees shall be provided for all projects subject to Type II design review, partition, or subdivision. Street trees shall be installed in accordance with the provisions of NMC 15.420.010(B)(4).*

Finding: Lot 07000 where the modular is being placed does not border any public streets. Adjacent lots under the same ownership and use are currently meeting the street tree requirements.

This criterion is met.

U. Street Lights. *All developments shall include underground electric service, light standards, wiring and lamps for streetlights according to the specifications and standards of the Newberg public works design and construction standards. The developer shall install all such facilities and make the necessary arrangements with the serving electric utility as approved by the city.*

Upon the city's acceptance of the public improvements associated with the development, the street lighting system, exclusive of utility-owned service lines, shall be and become property of the city unless otherwise designated by the city through agreement with a private utility.

Street lights were provided with a previous project.

This criterion is met.

Finding:

V. Transit Improvements. Development proposals for sites that include or are adjacent to existing or planned transit facilities, as shown in the Newberg transportation system plan or adopted local or regional transit plan, shall be required to provide any of the following, as applicable and required by the review authority:

1. Reasonably direct pedestrian connections between the transit facility and building entrances of the site. For the purpose of this section, "reasonably direct" means a route that does not deviate unnecessarily from a straight line or a route that does not involve a significant amount of out-of-direction travel for users.

2. A transit passenger landing pad accessible to disabled persons.

3. An easement of dedication for a passenger shelter or bench if such facility is in an adopted plan.

4. Lighting at the transit facility. [Ord. 2889 § 2 (Exh. B §§ 43 – 45), 12-6-21; Ord. 2880 § 2 (Exh. B §§ 51, 52), 6-7-21; Ord. 2871 § 3 (Exh. D), 3-1-21; Ord. 2862 § 1 (Exh. A § 1), 6-15-20; Ord. 2822 § 1 (Exh. A), 2-5-18; Ord. 2810 § 2 (Exhs. B, C), 12-19-16; Ord. 2763 § 1 (Exh. A § 19), 9-16-13; Ord. 2736 § 1 (Exh. A §§ 1, 3, 4), 3-21-11; Ord. 2619, 5-16-05; Ord. 2513, 8-2-99; Ord. 2507, 3-1-99; Ord. 2494, 4-6-98; Ord. 2451, 12-2-96. Code 2001 §§ 151.681, 151.683, 151.684 – 151.686, 151.689 – 151.692, 151.694, 151.695, 151.701 – 151.703, 151.705.]

Finding: The applicant is not proposing transit improvements and the site is not adjacent to existing or planned transit facilities.

This criterion is not applicable.

15.505.040 Public utility standards.

A. Purpose. *The purpose of this section is to provide adequate services and facilities appropriate to the scale and type of development.*

B. Applicability. *This section applies to all development where installation, extension or improvement of water, wastewater, or private utilities is required to serve the development or use of the subject property.*

C. General Standards.

1. The design and construction of all improvements within existing and proposed rights-of-way and easements, all improvements to be maintained by the city, and all improvements for which city approval is required shall conform to the Newberg public works design and construction standards and require a public improvements permit.

2. The location, design, installation and maintenance of all utility lines and facilities shall be carried out with minimum feasible disturbances of soil and site. Installation of all proposed public and private utilities shall be coordinated by the developer and be approved by the city to ensure the orderly extension of such utilities within public right-of-way and easements.

D. Standards for Water Improvements. All development that has a need for water service shall install the facilities pursuant to the requirements of the city and all of the following standards. Installation of such facilities shall be coordinated with the extension or improvement of necessary wastewater and stormwater facilities, as applicable.

1. All developments shall be required to be linked to existing water facilities adequately sized to serve their intended area by the construction of water distribution lines, reservoirs and pumping stations which connect to such water service facilities. All necessary easements required for the construction of these facilities shall be obtained by the developer and granted to the city pursuant to the requirements of the city.

2. Specific location, size and capacity of such facilities will be subject to the approval of the director with reference to the applicable water master plan. All water facilities shall conform with city pressure zones and shall be looped where necessary to provide adequate pressure and fire flows during peak demand at every point within the system in the development to which the water facilities will be connected. Installation costs shall remain entirely the developer's responsibility.

3. The design of the water facilities shall take into account provisions for the future extension beyond the development to serve adjacent properties, which, in the judgment of the city, cannot be feasibly served otherwise.

Finding: No new connection to the public water line is proposed. There is a hydrant at the entrance to the site. The applicant submitted a fire flow test performed by Wyatt Fire Protect Inc. These results need to be submitted with the building permit application to be reviewed by the Fire Marshall for approval.

4. Design, construction and material standards shall be as specified by the director for the construction of such public water facilities in the city.

This criterion will be met when the Fire Marshall determines flows are adequate for fire protection requirements.

E. Standards for Wastewater Improvements. All development that has a need for wastewater services shall install the facilities pursuant to the requirements of the city and all of the following standards. Installation of such facilities shall be coordinated with the extension or improvement of necessary water services and stormwater facilities, as applicable.

1. All septic tank systems and on-site sewage systems are prohibited. Existing septic systems must be abandoned or removed in accordance with Yamhill County standards.

2. All properties shall be provided with gravity service to the city wastewater system, except for lots that have unique topographic or other natural features that make gravity wastewater extension impractical as determined by the director. Where gravity service is impractical, the developer shall provide all necessary pumps/lift stations and other improvements, as determined by the director.

3. All developments shall be required to be linked to existing wastewater collection facilities adequately sized to serve their intended area by the construction of wastewater lines which connect to existing adequately sized wastewater facilities. All necessary easements required for the construction of these facilities shall be obtained by the developer and granted to the city pursuant to the requirements of the city.

4. Specific location, size and capacity of wastewater facilities will be subject to the approval of the director with reference to the applicable wastewater master plan. All wastewater facilities shall be sized to provide adequate capacity during peak flows from the entire area potentially served by such facilities. Installation costs shall remain entirely the developer's responsibility.

5. Temporary wastewater service facilities, including pumping stations, will be permitted only if the director approves the temporary facilities, and the developer provides for all facilities that are necessary for transition to permanent facilities.

6. The design of the wastewater facilities shall take into account provisions for the future extension beyond the development to serve upstream properties, which, in the judgment of the city, cannot be feasibly served otherwise.

7. Design, construction and material standards shall be as specified by the director for the construction of such wastewater facilities in the city.

Finding: No new connection to the public wastewater line is proposed. The site is connected to the public wastewater line currently.

This criterion is met.

F. Easements. Easements for public and private utilities shall be provided as deemed necessary by the city, special districts, and utility companies. Easements for special purpose uses shall be of a width deemed appropriate by the responsible agency. Such easements shall be recorded on easement forms approved by the city and designated on the final plat of all

subdivisions and partitions. Minimum required easement width and locations are as provided in the Newberg public works design and construction standards. [Ord. 2810 § 2 (Exhs. B, C), 12-19-16.]

Finding: N College Street is under the jurisdiction of the Oregon Department of Transportation (ODOT). There is a public utility easement benefiting ODOT along the property frontage.

15.505.050 Stormwater system standards.

This criterion is met.

A. Purpose. *The purpose of this section is to provide for the drainage of surface water from all development; to minimize erosion; and to reduce degradation of water quality due to sediments and pollutants in stormwater runoff.*

B. Applicability. *The provisions of this section apply to all developments subject to site development review or land division review and to the reconstruction or expansion of such developments that increases the flow or changes the point of discharge to the city stormwater system. Additionally, the provisions of this section shall apply to all drainage facilities that impact any public storm drain system, public right-of-way or public easement, including but not limited to off-street parking and loading areas.*

C. General Requirement. *All stormwater runoff shall be conveyed to a public storm wastewater or natural drainage channel having adequate capacity to carry the flow without overflowing or otherwise causing damage to public and/or private property. The developer shall pay all costs associated with designing and constructing the facilities necessary to meet this requirement.*

Finding: The proposed development will create more than 500 square feet of impervious area. The applicant has submitted a LIDA sizing form to determine the size of facility needed to treat and detain the 1,792 square feet of new impervious area. The applicant plans to increase the size of an existing facility to accommodate the new run off. The existing facility drains to the 12-inch private storm line which drains to a catch basin that connects to a public storm line in Pinehurst Drive. The applicant will be required to submit final plans with a LIDA sizing form with the building permit application.

This criterion will be met if the aforementioned condition of approval is adhered to.

D. Plan for Stormwater and Erosion Control. No construction of any facilities in a development included in subsection (B) of this section shall be permitted until an engineer registered in the State of Oregon prepares a stormwater report and erosion control plan for the project. This plan shall contain at a minimum:

- 1. The methods to be used to minimize the amount of runoff, sedimentation, and pollution created from the development both during and after construction.***
- 2. Plans for the construction of stormwater facilities and any other facilities that depict line sizes, profiles, construction specifications, and other such information as is necessary for the city to review the adequacy of the stormwater plans.***
- 3. Design calculations shall be submitted for all drainage facilities. These drainage calculations shall be included in the stormwater report and shall be stamped by a licensed professional engineer in the State of Oregon. Peak design discharges shall be computed based upon the design criteria outlined in the public works design and construction standards for the city.***

Finding: The stormwater management plan and erosion control plan submitted was prepared by a professional engineer licensed in the State of Oregon. The applicant will be required to obtain a City of Newberg Erosion Control Permit prior to any ground disturbing activity.

This criterion will be met if the aforementioned condition of approval is adhered to.

E. Development Standards. Development subject to this section shall be planned, designed, constructed, and maintained in compliance with the Newberg public works design and construction standards. (Ord. 2810 § 2 (Exhs. B, C), 12-19-16.)

Finding: Because the applicant has not provided construction plans, the applicant will be required to provide detailed construction plans that address requirements outlined in the Public Works Design and Construction Standards in accordance with NMC 13.25 Stormwater Management.

A private maintenance agreement for the stormwater facility will be required. Because a private maintenance agreement for the stormwater facility has not been recorded, the applicant will submit a private maintenance agreement for the onsite stormwater facility and have the approved agreement recorded.

This criterion will be met if the aforementioned condition of approval is adhered to.



COMMUNITY DEVELOPMENT LAND USE APPLICATION REFERRAL

The enclosed material has been referred to you for your information and comment. Any comments you wish to make should be returned to the Community Development Department prior to: May 9, 2023
Please refer questions and comments to: Clay Downing

NOTE: Full size plans are available at the Community Development Department Office.

APPLICANT: Yamhill Land Development Services LLC
REQUEST: Placement of Modular Classroom
SITE ADDRESS: 1605 N College St
LOCATION: N/A
TAX LOT: R3218AD 07000
FILE NO: DR223-0002/ADJC23-0004
ZONE: R-1(Residential Low Density Zoning
HEARING DATE: N/A



Project Information is Attached:

- Reviewed, no conflict.
- Reviewed; recommend denial for the following reasons:
- Require additional information to review. (Please list information required)
- Meeting requested.
- Comments. (Attach additional pages as needed)

Reviewed By:

Scott Albert - Ziplly Fiber Network Engineer
Organization: 503-526-3544 scott.albert@ziply.com

4/25/23

Date: